

**Appendix 1: Review of Studies Submitted by Objectors**

**Cambridge Inner Green Belt:  
Review of Studies Submitted by Objectors**

November 2015

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*This document has been prepared and checked in accordance with ISO 9001:2008.*

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## **1.0 Introduction**

### **1.1 Appointment and Scope**

1.1.1. This report is supplemental to the Cambridge Inner Green Belt Boundary Study by LDA Design (October 2015) (referred to in this report as LDA Design’s main report). As described in section 1.1 of that report, Cambridge City Council and South Cambridgeshire District Council (jointly referred to in this report as the Councils) commissioned LDA Design to undertake two pieces of work:

- 1) To undertake assessment of the Inner Green Belt Boundary and set out the methodology used. The assessment should provide a robust, transparent and clear understanding of how the land in the Cambridge Green Belt performs against the purposes of the Cambridge Green Belt.
- 2) To review the methodologies put forward by objectors in relation to the Inner Green Belt Boundary.

1.1.2. This report addresses item 2. Item 1 is the subject of LDA Design’s main report.

1.1.3. The following studies are reviewed in this report:

- Response to Review of the Inner Green Belt Boundary Study, CSa Environmental Planning (February 2013) on behalf of Januarys (Cambridge) Limited for North of Barton Road Landowners’ Group and South of Barton Road Landowners’ Group.
- Cambridge Green Belt Review, Terence O’Rourke Limited (July 2012) on behalf of Grosvenor Estates.
- Green Belt Review & Critique of Cambridge South, Helen Thompson and Bidwells (September 2013) on behalf of Lands Improvement Holdings Limited, Pigeon Land, Jesus College and the Pemberton Trust.
- Cambridge South East Green Belt Review Technical Report, Tyler Grange (September 2013) on behalf of Commercial Estates Group.
- Green Belt Assessment: Land at Fen Ditton, Cambridge, Liz Lake Associates (September 2013) on behalf of Carter Jonas for the Quy Estate.

1.1.4. The studies are reviewed in turn in the following sections of this report. The section titles refer to the promoters who commissioned the reports.

### **1.2 Assessment Parcels**

1.2.1. The assessments undertaken on behalf of Lands Improvement Holdings Limited and Commercial Estates Group both break down the areas of land to be assessed into small parcels, sometimes comprising a single field or part of a field. This is not appropriate.

1.2.2. The issues raised by Green Belt Purposes, such as urban sprawl, merging of settlements, setting, etc., are matters that must be considered at a broad scale; they require a holistic, contextual view to be taken and do not stop at specific field boundaries. Applying the various criteria involved in a Green Belt assessment may

result in one field receiving a slightly different score from the adjacent field. If the consequence is that one field is considered appropriate for development, that development would change the conditions in the neighbouring field and would be likely to change the results of a future Green Belt assessment of the neighbouring field. Whilst the harm resulting from the incremental development of each individual field might be relatively minor, the harm resulting from what might add up to a significant loss of Green Belt land could be much greater.

- 1.2.3. It is therefore important to keep sight of the 'big picture', considering each area of Green Belt land under assessment in the context of the city as a whole.
- 1.2.4. For these reasons, the Study undertaken by LDA Design defined larger sectors that can be considered in relation to the city as a whole and enable robust conclusions to be drawn regarding the performance of Green Belt Purposes. Sectors are only sub-divided where there are areas of noticeably different land use, character or context which could change the outcome of the assessment to a material degree.

## 2.0 North and South of Barton Road Landowners' Groups

- 2.1.1. This report relates to land north and south of Barton Road, equivalent to sectors 3 and 4 in LDA Design's main report.
- 2.1.2. The report does not itself contain a Green Belt Assessment, nor does it propose a methodology for one. Rather, it comments on the Councils' 2012 Inner Green Belt Boundary Study (the Councils' Study) and refers to an initial landscape and visual impact assessment previously undertaken by CSa in relation to the promotion site.
- 2.1.3. In criticising the Councils' Study, the report places considerable emphasis on the issue of coalescence between settlements, asserting in several places (paras 2.8, 2.11 and 3.4) that this is the main purpose of Green Belt. Such an assertion is at odds with Green Belt Policy as contained in the NPPF, which sets out five purposes in paragraph 80 and states in paragraph 79 that *'the fundamental aim of Green Belt Policy is to prevent urban sprawl by keeping land permanently open'*.
- 2.1.4. At paragraph 2.9, the report states that the Councils' methodology places too much weight on matters such as rural character, which is not in itself a Green Belt matter. However, as identified in LDA Design's main report, rural character is a key element of the setting and special character of Cambridge, the preservation of which is covered by National Green Belt purpose 4.
- 2.1.5. At paragraph 2.10, the report states *'Whilst the land does not form part of a larger gap between the two settlements, it is the M11 which provides a clear break between Coton and Cambridge, thus preventing any perceived or actual coalescence'*. This appears to suggest that Coton and Cambridge could both extend as far as the M11 without perceived or actual coalescence because the M11 would separate them. It is apparent at Girton, Histon and Milton on the north edge of Cambridge that the separation created by a major road between two otherwise conjoined areas of development is minimal and that the distinctive identity and rural setting of necklace villages is significantly compromised in such circumstances.
- 2.1.6. At paragraph 2.12, the report states that the Councils' Study does not identify any significant views adjacent to the site. However, Figure 3 in the Councils' Study identifies a significant view from Coton Countryside Reserve towards the site. This view is represented as Photograph 1 on Figure 15 to LDA Design's main report (also the cover image to that report) in which the importance of sector 3 as the rural foreground to the city is clear.
- 2.1.7. Paragraph 2.14 assesses the land north of Barton Road as being of low to medium landscape quality. Even taken on its own terms, this does not appear to recognise the value of the strong field pattern and substantial hedgerows within the land. More significantly, it fails to recognise the value of the land in providing the rural setting to the west side of Cambridge and being the closest area of rural land to the historic core.
- 2.1.8. At paragraph 1.4, the study summarises the findings of the initial landscape and visual impact assessment. The second bullet point states that significant views *'could be*

*retained along green corridors or above new buildings, with some open land retained in the foreground'. The cover photograph and Photographs 1 and 2 on Figure 15 to LDA Design's main report demonstrate that for the significant views to be retained intact, with the land forming the rural foreground to the city, the land must be retained entirely free of new development.*

### 3.0 Grosvenor Estates

- 3.1.1. This study relates to land between Trumpington and the M11 on the west side of Hauxton Road, identified as sector 7 in LDA Design's main report.
- 3.1.2. Note: 20 Figures are listed on page 2 of the report but Figures 18-20 were missing from the report as reviewed.
- 3.1.3. Reference is also made to Terence O'Rourke drawing number 173604/SK/600 (included as Appendix 1 to this report and originally submitted to the City Council as a part of Grosvenor's representation 27137 to the Proposed Submission consultation in relation to Policy 4: The Cambridge Green Belt), which provides an indication of Grosvenor's proposals for an extension to the consented Trumpington Meadows development.
- 3.1.4. The report refers at section 1.1 to the Cambridge Green Belt purposes (as defined in LDA Design's main report) and makes numerous references to LDA Design's Cambridge Green Belt Study 2002, undertaken on behalf of South Cambridgeshire District Council.
- 3.1.5. Section one (numbered as paragraph 2.1 onwards) contains extensive baseline information, focussing particularly on landscape character and views.
- 3.1.6. Section two (numbered as paragraph 3.1 onwards) analyses the effects of development.
- 3.1.7. Section three (numbered as paragraph 4.1 onwards) presents the conclusions of the assessment.
- 3.1.8. Paragraph 3.4 refers to the Special Qualities of Cambridge and its surrounding landscape that were identified in section 6 of LDA Design's 2002 Study. The report lists 11 of the 14 Special Qualities identified by LDA Design, describing them as '*qualities relevant to this study*'. The three it omits are:
- A large historic core relative to the size of the city as a whole
  - A city of human scale easily crossed by foot and by bicycle
  - Designated sites and areas enriching the setting of Cambridge.
- 3.1.9. There is no explanation as to why these three qualities are not mentioned. As the assessment of sector 7 in LDA Design's main report shows, they are of relevance to the sector.
- 3.1.10. Having listed 11 of the qualities, the report makes little reference to the majority of them. Only six of those listed are included in the table at paragraph 4.6 (see below).
- 3.1.11. Paragraphs 3.10 onwards contain an assessment of the effects of development on character areas. As the heading above paragraph 3.7 indicates, this is undertaken very much as an assessment of effects on landscape and visual resources rather than an assessment of effects relevant to Green Belt Purposes.

- 3.1.12. At paragraph 3.20, the report states that any future development would predominantly be located in an area classified in LDA Design's 2002 Study as Connective landscape. This refers to the assessment of townscape and landscape role and function in section 5.6 of LDA Design's 2002 Study, parts of which are quoted in paragraph 3.6. Whilst it is correct that LDA Design's 2002 Study identified much of the landscape on which Grosvenor propose development as Connective, it identified some of it as Supportive. Due to the significant changes that have occurred since 2002 in this area of Cambridge, associated with the construction of Addenbrooke's Road and the Glebe Farm and Trumpington Meadows developments, the whole of sector 7 is now classified as Supportive landscape for the reasons explained in section 4.14 of LDA Design's main report.
- 3.1.13. At paragraph 3.21, the report states that any extension to Trumpington Meadows would lie in the same character areas as land previously released from the Green Belt, which was subject to detailed analysis work undertaken for the Trumpington Meadows outline planning application. Whilst it is correct that the land lies in the same landscape character area, landscape character is a component of the baseline understanding of the landscape but is not directly relevant to Green Belt Purposes.
- 3.1.14. Paragraph 3.23 states *'It would be important that a green swathe of open landscape around any new development edge is maintained north of the M11 so as to retain a strong landscape setting on the approach to Cambridge along Hauxton Road and in views from the M11'*. There is further reference to a green swathe in paragraph 4.11. However, there is reference to sports pitches in the table at paragraph 4.6, and paragraph 4.15 also refers to sports pitches. Terence O'Rourke drawing number 173604/SK/600 (see Appendix 1) shows that the land between the proposed extension to Trumpington Meadows and the M11 would be allocated for outdoor sport. It therefore appears clear that Grosvenor's proposals would not retain agricultural land between the M11 and the proposed Trumpington Meadows extension.
- 3.1.15. At paragraph 3.41, the report states that the experience and character of views from the western section of the Country Park and from the A10 would remain unaltered. Once the consented Trumpington Meadows scheme is completed, a significant area of agricultural land will remain between the built edge and the M11 junction. Under the Grosvenor proposals, approximately half of this agricultural land would be built upon and the remainder would change from agricultural land to sports pitches with associated fencing and lighting. This would have a significant effect on the character and appearance of the land and would affect the perception of the city as being set in a landscape which retains a strongly rural character, this being one of the important qualities of the Green Belt identified in section 5 of LDA Design's main report (and a Special Quality identified in LDA Design's 2002 Study).
- 3.1.16. In the unnumbered paragraph after 3.47, the report states that there will be no impact on four attributes relevant to Green Belt Purposes. Whilst it is correct that views of the historic core of Cambridge would remain unaffected and that the green corridor along the River Cam would remain intact, LDA Design's assessment of sector 7 is that significant expansion of development in this area would increase the impression of

urban sprawl and could start to threaten the compactness of the city. The report states that *'a green setting to the city can be maintained'*; as noted above, whilst sports pitches could be described as green, there would be an effect on the rural setting of the city, and it is the rural character of the landscape that is important to Green Belt Purposes, as emphasised in the references to LDA Design's 2002 Study quoted at paragraphs 4.2 and 4.3 of the report.

- 3.1.17. The table at paragraph 4.6 purports to consider the effect of development on various qualities that contribute to Green Belt Purposes. Only six of the 14 qualities identified in LDA Design's 2002 Study are considered, along with four qualities drawn from other sources. In each case, the table states that there will be no impact on the relevant quality but the accompanying comments do not rigorously support this and several amount to little more than bare assertion. Against 'A distinct urban edge', there is a statement that development of a stadium has potential to create a truly distinct and exemplar urban gateway. There does not appear to be any other reference in the report to a stadium, and a building of the scale of a stadium would appear to conflict with the building heights strategy described in paragraph 4.12. A stadium would also be completely out of character with the new residential urban edge that is being created at Glebe Farm and Trumpington Meadows. It would presumably require extensive areas of car parking which would further detract from urban form and the rural character of the landscape adjoining the urban edge.
- 3.1.18. Against the quality 'A city set in a landscape which retains a strongly rural character', it is noted that sports pitches are located within the Green Belt to the west of Cambridge and do not impact on the overall rural setting to the city. This occurs in sectors 3.3 and 4.3 assessed in LDA Design's main report, both of which have a strong framework of large hedgerows and trees which contain and conceal the pitches so that they are not perceived within the otherwise rural character of the landscape. This would not be the case in sector 7.
- 3.1.19. Paragraph 4.7 refers to the classification of the landscape as Connective. As discussed above in relation to paragraph 3.20, LDA Design's 2002 Study identified some of the landscape on which Grosvenor propose development as Supportive. Due to the changes that have occurred in the area, LDA Design's main report now classifies the whole of sector 7 as Supportive.
- 3.1.20. In conclusion, the report does not adopt a methodology that amounts to a rigorous assessment of the performance of the land or the implications of its development for Green Belt purposes. The methodology largely follows that used for Landscape and Visual Impact Assessments, focussing on effects on landscape character and views. Where matters directly relevant to Green Belt are mentioned, they are not considered in a robust or transparent manner and the conclusions in relation to Green Belt are largely unsubstantiated.

## 4.0 Pigeon Land, Lands Improvement Holdings Limited and Others

- 4.1.1. This study relates to land south of Trumpington, identified as sector 8 in LDA Design's main report.
- 4.1.2. The study includes a detailed review and critique of the Councils' Study and goes on to undertake an assessment of the land described as Cambridge South using the same methodology as used in the Councils' Study but with two adjustments, as explained in paragraph 5.12. Paragraph 5.9 provides a justification for the first adjustment (using smaller areas) but, as explained in section 1.2 of the present report, dividing the assessment area into small land parcels is not a valid approach. The second adjustment to the Councils' methodology, that Importance to Green Belt is based on the highest value of the preceding attributes, is justified in paragraph 4.33. Since the evaluation process is not clear in the Councils' Study, this approach does not appear unreasonable.
- 4.1.3. Whilst stating that the study follows the Councils' methodology (subject to the two adjustments above), it includes two tables which do not appear in the Councils' study and appear to supplement the methodology.
- 4.1.4. Table 2 shows the Inter-relationship of Purposes and Assessment. Under the heading 'Purposes', it sets out the five National Green Belt purposes and the three Cambridge Green Belt purposes (both terms as defined in LDA Design's main report), as well as the factors identified in the South Cambridgeshire Core Strategy 2007 as contributing to the special character of Cambridge. Under the heading 'Method of Assessment', it lists the four 'purposes' set out in paragraph 2.2 of the Councils' Study and it indicates how the Purposes are to be assessed. The table states that National Green Belt purposes 1, 3 and 5 are not included in the Councils' Assessment as they are not usually seen to be of paramount importance for Cambridge. In relation to National Green Belt purpose 4 (setting and special character) a number of factors are identified which it is assumed the Study takes into account in the assessment. There is repeated emphasis on the rural character of the landscape both as the setting to the city and the setting of necklace villages. Reference is also made to soft green edges to the city, historic and social associations and the separation, setting, scale and character of villages. There is acknowledgement that Cambridge is a compact city but the text states that *'care must be exercised as this is not necessarily about distance from the centre but about ease of access and communication between different areas'*. However, although compactness is mentioned here, it does not appear to be considered or addressed in the subsequent assessment.
- 4.1.5. Table 3 sets out how the Assessment of Special Character has been approached. It draws from the Cambridge Landscape Character Assessment 2003 (CLCA) and LDA Design's 2002 Study. In relation to the CLCA, Defining Character is expressed to be in terms of setting, separation, views, green corridor and environmental character. However, from reviewing the CLCA, environmental character does not appear to be identified in that document as Defining Character. The table then highlights relevant aspects of Supporting Character from the CLCA. In relation to Edges, it notes that the

south extent of the urban area is identified in the CLCA as a negative edge but does not refer to the new urban edge which has been created along Addenbrooke's Road and which will, as the vegetation matures, become a positive edge. One of the aspects of Supporting Character in the CLCA is Archaeology but this is omitted from the table despite the fact that the Cambridge South site includes a Scheduled Monument, as identified as paragraph 5.21 of the report.

4.1.6. In relation to LDA Design's 2002 Study, the table mentions a number of the Special Qualities identified in section 6 of that Study but omits to mention eight of the 14 Special Qualities. Those omitted are as follows:

- A large historic core relative to the size of the city as a whole
- A city focussed on the historic core
- Short and/or characteristic approaches to Cambridge from the edge of the city
- A city of human scale easily crossed by foot and by bicycle
- Topography providing a framework to Cambridge
- Designated sites and areas enriching the setting of Cambridge
- Long distance footpaths and bridleways providing links between Cambridge and the open countryside
- A city set in a landscape which retains a strongly rural character.

4.1.7. As is apparent from the assessment of sector 8 in LDA Design's main report, these omitted qualities cover a number of considerations which are important to the assessment of this sector.

4.1.8. There are a number of errors and inaccuracies in the table in relation to the Special Qualities that are referred to:

- The table states that no particular qualities are identified to be safeguarded on plan 1641LP/09 in LDA Design's 2002 Study. However, the plan identifies the land as being important in separating Hauxton and Little Shelford from Cambridge.
- The table states that the site is in an area of Connective landscape/townscape. Whilst this was correct in the 2002 Study, the assessment in LDA Design's main report identifies the northern part of sector 8 as Supportive landscape for the reasons explained in section 4.14 of LDA Design's main report.
- In relation to key views, the table states that Cambridge South does not come into the view cone of the view from the M11 junction. Whilst it is no longer identified as a key view in LDA Design's main report, the view from the M11 junction is an important part of the experience of arrival at Cambridge along Hauxton Road and the Cambridge South site provides the rural setting on the right hand side of the view. A key view has been identified in LDA Design's main report from St Margaret's Mount (Rowley's Hill). The Cambridge South site forms part of the rural foreground to the city in this view.

- In relation to the distribution, physical separation, setting, scale and character of necklace villages, the table states that plan 1641LP/09 shows open countryside separating the site from Little Shelford and from Hauxton. This wording is incorrect. The plan shows the site as open countryside separating the inner necklace villages from Cambridge.
- In relation to the attribute of a soft green edge to the city, the table states that Cambridge South is bounded on three sides with road infrastructure making it a hard and negative edge. The city edge is defined by Addenbrooke’s Road and by the ribbon development along Cambridge/Shelford Road. The latter has a soft green edge. The planting that has taken place in association with the Glebe Farm development is expected to provide a soft green edge along Addenbrooke’s Road as it matures.

4.1.9. Paragraph 5.27 of the report proposes a new Green Belt boundary extending out to the M11 and the edge of the river corridor, which appears to be in order to align the boundary with permanent features. Paragraph 5.28 states that not all land released from the Green Belt need be developed and that *‘The apron of land which the Councils regard as important to the setting of Cambridge could be established’*. If the premise for a Green Belt release is, as implied at paragraph 6.6, that much of the site *‘could be developed without significant undue harm to the purpose of the Green Belt’*, then the new development edge could form a permanent boundary to the Green Belt. Land which is regarded as important to the setting of Cambridge should not be released from the Green Belt.

4.1.10. The assessment of land in the sector is presented in Table 4, with the site broken down into fourteen areas as shown on Figure 12 to the report. Table 4 contains minimal explanation to support the assessment and there appear to be significant grounds for concern. These are listed below against the relevant criteria in the left hand column of table 4.

|                            |   |
|----------------------------|---|
| Character Area/Type        | For most of the areas, the table states that parts are Supporting but the reason for this is unclear. Table 3 appears to indicate that no elements of Supporting Character are applicable to sector 8. Areas 6 and 9 are stated to be wholly or partly Defining, which is assumed to relate to the river corridor.  |
| Proximity to Historic Core | There appears to be inaccuracy in the figures for many of the areas. For example, both areas 1 and 4 are stated to be 5km from the historic core but area 4 lies 1km south of area 1 and is therefore 1km further from the historic core. Area 6, stated to be 5.5km, is 1.5km south of area 1. Area 14 is stated to be 5.5km whilst area 12, which is immediately adjacent to it, is stated to be 5km. |
| Edge Type                  | For areas 1, 12, 13 and 14, edge type is described in relation to the adjoining roads. However, the relevance of edge type  |

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|  | is in relation to the quality of the urban edge of the city, which for these areas is defined by the new housing development on the north side of Addenbrooke’s Road.  |
| Prevalent Local Built Form   | In areas 1, 12, 13 and 14, there is no reference to the new housing along Addenbrooke’s Road.  |
| Importance to Setting  | Whilst paragraph 5.16 of the report refers to the Guidance Notes in Table 2 of the Councils’ Study, there is no explanation as to how the various areas have been assessed. Areas 1-5 and 7-8 are assessed as being of Low importance to setting although, as agricultural land, they all contribute to the rural setting of the city, which is a quality mentioned in the Table 2 Guidance Notes in the Councils’ Study. Areas 10-13 are identified as being of Medium importance, which may be due to their greater visibility from adjacent roads (M11 and Hauxton Road), although area 1, which is adjacent to Addenbrooke’s Road, is classified as being of Low importance. Area 14 is classified as High importance, although there is no apparent reason why it should differ from area 12. |
| Importance to Character  | Similarly, there is no explanation of these assessments. A clear anomaly is that areas 13 and 14 are classified as Medium, whilst area 12, which lies between them, is classified as Low.  |
| Importance to Physical Separation, Distribution, Setting, Scale and Character of Green Belt Villages | Areas 1-4 are classified as Negligible or Low, which does not appear to take into account the implications of additional development alongside the existing ribbon development on Cambridge/Shelford Road, the effect of which would be to decrease the perception of separation between Cambridge and Great Shelford. Areas 10-14 are classified as Low or Negligible, although they form part of the rural separation between Cambridge and Hauxton.   |
| Importance to Rural Character  | Most of the areas are assessed as Medium, with area 1 being Low and areas 6 and 9 being High. As noted in the assessment of sector 8 in LDA Design’s main report, there are urbanising influences on the sector but, being entirely agricultural land and with a visual connection to the countryside beyond, it retains much of its rural character. The assessments of Low or Medium for the majority of this sector downgrade its importance unduly.  |
| Importance to Green Belt   | The assessment copies the highest score given against the previous four criteria, with the highest scoring criterion   |

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|  | <p>varying from one area to another. The concerns expressed above are reflected in the assessments. Areas 5, 6, 7, 9 and 14 are assessed as being of High or Very High importance. With the exception of area 14, these are the southernmost areas within the sector. This ignores the importance of the northern part of the sector in forming the setting for the new edge of the city being constructed at Glebe Farm and the greater visibility of the northern part of the sector in views on the approach to Cambridge along Hauxton Road and from higher ground to the west such as St Margaret’s Mount (Rowley’s Hill).</p> |
| <p>Significance of Development on the Green Belt</p> | <p>It is assumed that this assessment uses the Significance Matrix at Table 1 of the Councils’ Study. Since the magnitude of effect of a development proposal is not stated in the report, it must be worked out from the matrix. This appears to indicate that a Very High magnitude has been identified for areas 9 and 14, High for 1, 6 and 10-13, and Medium for areas 2-5 and 7-8. The rationale behind these assessments is not at all apparent.</p>   |

4.1.11. Paragraph 6.6 of the report states that most of the land in the sector is of Medium importance to the purposes of Green Belt and ‘*could be developed without significant undue harm to the purpose of the Green Belt*’. Notwithstanding the concerns expressed above in relation to the assessment, no explanation or justification is provided as to why land of Medium importance to Green Belt can be developed without significant undue harm to Green Belt purposes. Since this appears to be the conclusion upon which the entire assessment turns, the lack of any explanation is highly surprising.

4.1.12. In conclusion, there are serious concerns about the study. The methodology appears to overlook a number of matters that should be taken into consideration in the assessment, which are identified in documents from which the study purports to draw. The assessment is based on inappropriately small land parcels. It lacks any explanation of the judgements made and there are obvious errors and inaccuracies in some of the judgements.

## 5.0 Commercial Estates Group

- 5.1.1. This report was prepared by Tyler Grange on behalf of the promoter of land at South East Cambridge comprising sectors 11, 12 and the westernmost part of 13 in LDA Design's main report.
- 5.1.2. The report contains a critique of the Councils' Study and then presents what it describes as a '*robust and transparent*' methodology which is used to assess not only South East Cambridge but also selected other sectors of the Inner Green Belt to contrast its conclusions with those of the Councils.
- 5.1.3. Paragraph 19 on page 13 refers to '*a methodology that places equal weighting to a range of Green Belt criteria*', and the approach of combining the outcomes against all criteria is used throughout the assessments. The criteria are derived from five purposes of the Cambridge Green Belt identified by Tyler Grange, which are set out at paragraph 42. They include National Green Belt purposes 1 and 4 (with the wording of 4 modified to refer specifically to Cambridge) and Cambridge Green Belt Purposes 1 and 3. In addition there is a specific purpose relating to the protection of green corridors running from open countryside into the urban area, which is one of the 'purposes' identified in section 2.2 of the Councils' Study. There is apparent duplication between the reference to special character in National Green Belt purpose 4 and the unique character of Cambridge in Cambridge Green Belt purpose 1. On the other hand, Cambridge Green Belt purpose 2, which relates purely to setting, is omitted and the only reference to setting is under National Green Belt purpose 4 where it is combined with special character.
- 5.1.4. Although not mentioned in paragraph 42, National Green Belt purpose 3 is also used in the assessments of the sectors.
- 5.1.5. Paragraphs 48-51 explain that the sectors are sub-divided into distinct land parcels. Section 1.2 of this report explains why such an approach is not appropriate.
- 5.1.6. The table in Appendix 2 lists the Green Belt purposes to be used for the assessments (i.e. those identified in paragraph 42 along with National Green Belt purpose 3) and identifies criteria for assessment against each purpose. It then identifies indicators to be used for assessing the various criteria. The criteria and indicators are largely drawn from existing sources, including the Special Qualities identified in LDA Design's 2002 Study. However, the following Special Qualities are not represented:
- A city focussed on the historic core
  - A city of human scale easily crossed by foot and by bicycle
  - Significant areas of distinctive and supportive townscape and landscape
  - Designated sites and areas enriching the setting of Cambridge
  - Long distance footpaths and bridleways providing links between Cambridge and the open countryside.

- 5.1.7. Three other Special Qualities are nominally represented but are used to a limited or minimal extent:
- Topography providing a framework to Cambridge – this is a key factor in South East Cambridge; it is used to a limited extent in considering views and in determining whether landform contains the land parcel under assessment but not as a quality in its own right.
  - Elements and features contributing positively to the character of the landscape setting – this is referred to in relation to setting and special character but is not addressed by any of the indicators.
  - The distribution, physical separation, setting, scale and character of necklace villages – physical separation is addressed by the criteria and indicators but the other aspects of this Special Quality are not considered.
- 5.1.8. The table in Appendix 3 sets out scoring parameters against each of the assessment indicators.
- 5.1.9. In relation to the purpose of safeguarding countryside from encroachment, one of the criteria identified in Appendices 2 and 3 is *'To what extent is the land parcel contained/separated from the wider countryside by landform?'*. The parameters in Appendix 3 make clear that a land parcel will receive a low score for importance to Green Belt purposes if it is located on land which slopes towards the built edge. The relevance of this criterion to encroachment on the countryside is by no means clear. As sectors 10-13 are the only part of the Inner Green Belt where there is significant landform and the landform in this area slopes towards the urban edge, this criterion would appear to be particularly favourable to South East Cambridge.
- 5.1.10. One of the criteria in relation to setting and special character is *'Does the land parcel form part of a rural backdrop in views from within the city?'*. The indicator for this criterion is analysis of views from Castle Mound within the historic core. Both the criterion and the indicator are too limiting. Land in the Inner Green Belt, particularly the foothills of the Gog Magog Hills, which include the land at South East Cambridge, are important not just in views from Castle Mound but in views out to the countryside from locations within the south eastern areas of the city and in views across the city from other Green Belt areas. These issues are not given consideration.
- 5.1.11. The criteria identified for assessing the purpose of checking unrestricted sprawl are limited, referring only to the land parcel's role in halting ribbon development and whether the land parcel is closely associated with the existing built edge. As identified in the table in section 5.1 of LDA Design's main report, other criteria are relevant to the issue of sprawl including those related to compactness, urban structure and city edges, and elements and features contributing to the structure of the landscape. These qualities are not assessed.
- 5.1.12. The principle of giving equal weight to all six of the purposes in the assessments is inherently flawed for two reasons:

- 1) Since one of the purposes relates to green corridors, sectors which are not located within or adjacent to a green corridor will receive an assessment of low importance in relation to this purpose, which will bring down their overall score. A sector or land parcel which scored highly in relation to all five of the other purposes would receive a lower overall score than a sector which scored highly in the other five purposes and was in addition located within a green corridor. Whilst the importance of green corridors has been recognised in various publications in relation to the Cambridge Green Belt, there has never been a suggestion that Green Belt land within a green corridor is inherently more valuable than Green Belt land which is not in a green corridor.
- 2) Averaging the score of a sector or land parcel across all purposes implies that all areas of Green Belt land should ideally fulfil all Green Belt purposes. There has never been a suggestion in planning policy that this should be the case. For example, it is possible to envisage an area of Green Belt land that is critical in preventing coalescence between settlements but performs little other role in relation to the Green Belt purposes. The fact that it is critical in preventing coalescence could, on its own, mean that it is important to retain the land as Green Belt. However, if a high score for coalescence was combined with low scores against other Green Belt purposes and all were given equal weighting, its overall score would be low or medium-low, implying there is little reason to retain it as Green Belt.

5.1.13. Paragraphs 25-27 refer to two areas of the Inner Green Belt that have been excluded from the Study following representations from English Heritage (now Historic England) concerning their value and importance. These areas include land at Grantchester, which broadly corresponds to sectors 3-6 of the Councils' study, and land at Fen Ditton, which corresponds to Sector 18 of the Councils' study. Paragraph 28 states *'Land within these sectors is considered to be highly sensitive to change and important in respect of the Green Belt Purposes and has been scoped out of this review'*.

5.1.14. In order to test the validity of the CEG methodology, LDA Design has assessed parcels of land within these areas using the CEG methodology. Sub areas 4.1, 4.2 and 4.3 in Sector 4 (as identified in LDA Design's main report) have been assessed to represent land at Grantchester and sub areas 18.1, 18.2 and 19.1 have been assessed to represent land at Fen Ditton. The assessments are included in Appendix 2 to this report.

5.1.15. As indicated on Figures 23 and 24 within Appendix 2 to this report, the results of the assessment of land within South West Cambridge (land at Grantchester) indicate that only sub area 4.1 makes a High contribution overall to the Green Belt. It makes a High contribution to the Green Belt purposes of 'Ensure the protection of green corridors running from open countryside into the urban area', 'Assist in safeguarding the countryside from encroachment' and 'To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre', whilst making a Medium or Low contribution to the other purposes. Sub areas 4.2 and 4.3 both make a Medium contribution overall to the Green Belt, with sub area 4.2 making a High contribution to the Green Belt purpose of 'To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre', but both of these sub areas

making a Low contribution to the Green Belt purpose of 'Ensure the protection of green corridors running from open countryside into the urban area'.

- 5.1.16. As indicated on Figures 27 and 28 within Appendix 2 to this report, the results of the assessment of land within North East Cambridge (land at Fen Ditton) indicate that sub area 18.1 makes a Medium contribution overall to the Green Belt, whilst sub areas 18.2 and 19.1 make a Low contribution overall to the Green Belt. Sub area 18.1 makes a High contribution to the Green Belt purpose of 'Prevent communities in the environs of Cambridge from merging into one another and with the City' due to its location between Cambridge and Fen Ditton, and a Low contribution to the Green Belt purposes of 'Ensure the protection of green corridors running from open countryside into the urban area' and 'To preserve the setting and special character of Cambridge'. Sub areas 18.2 and 19.1 make a Low contribution to four out of six of the CEG Green Belt purposes, due to their location away from the existing edge of Cambridge but proximate to Fen Ditton and their location away from key views.
- 5.1.17. Whilst sub areas 4.2, 4.3, 18.1, 18.2 and 19.1 are within areas of land described in the CEG report as '*highly sensitive to change and important in respect of the Green Belt Purposes*', the application of the CEG methodology to these sub areas results in an assessment that they all make Medium or Low contributions to Green Belt purposes overall. This confirms that the methodology is fundamentally flawed.
- 5.1.18. Paragraphs 62-72 set out observations summarising the detailed assessments of various sectors of the Inner Green Belt. Paragraph 65 states that the overall assessment of sector 1 is Low to Medium due to generally low scores against three of the identified Green Belt purposes. However, the highlighted text box at the top of page 22 states that if the other three purposes were considered alone, the overall assessment would be Medium to High. The report therefore appears to be ignoring its own preference for equal weighting and instead emphasising the purposes which are of most importance in relation to the sector.
- 5.1.19. The same approach is taken in relation to sectors 7 and 8, where emphasis is placed on three purposes where the sectors score relatively highly stating that, in relation to those three purposes, the sectors would have an overall Medium to High score. However, the assessment shows that the overall score for most land parcels within the sectors is Medium and for some parcels it is Low.
- 5.1.20. However, in relation to South East Cambridge, at paragraph 69 the report criticises the Councils' Study for not emphasising the assessment of Low importance made in relation to separation and coalescence issues. The highlighted text in the box under paragraph 70 emphasises the low overall score for sectors 11 and 12, whereas the higher scores for particular purposes are emphasised for the previous sectors.
- 5.1.21. The detailed assessments of the sectors are contained in Appendices 8-11. Only Appendix 10 relating to South East Cambridge has been examined in any detail. A number of anomalies arise (the following may not be exhaustive):

- In response to the criterion *'Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?'*, for land parcels 2-6 the assessment is that they do not lie within any area recognised as a green corridor and they are given a score of Low. Against the question *'Does the land parcel create a soft green edge to the city or have a distinctive urban edge?'*, the answer for land parcel 8 is that it does not adjoin the built edge or urban fringe development and it is given a score of High. There is therefore an inconsistency in the way a land parcel is scored when it does not contribute to a particular criterion. The same inconsistency is apparent in other assessments, for example East Cambridge land parcel 16.
- In relation to land parcel 7, in response to the criterion *'Is the land parcel strongly rural in character?'* the parcel is scored as Medium, with the assessment referring to detractors such as the incinerator chimneys at Addenbrooke's Hospital and Marshalls buildings at Cambridge Airport. These detracting features are at a considerable distance from the land parcel and could not be said to undermine its strongly rural character.
- Also in relation to land parcel 7, the assessment of the comparison of the distance of the outer edge of the land parcel from the historic core with the existing extents of the built edge of Cambridge is misleading. It states that *'The outer extent of the land parcel lies a greater distance from the historic centre than the existing built edge at South East Cambridge but lies within the extents of the urban gateway on Cambridge Road'*. It is given a score of Medium. Drawing 1665/P10d shows that approximately half of parcel 7 lies beyond the extent of the existing built edge, and this is measured at the east side of Cherry Hinton, which is the furthest existing built edge from the historic core. The score of Medium is clearly incorrect in this instance and should be scored as High. The second indicator relevant to the issue of compactness relates to urban gateways and is scored as Low because the parcel does not lie adjacent to a main approach road with an identified gateway. Combining the Medium and Low scores against the two indicators, the land parcel is given an overall score of Low in relation to the purpose of preserving the unique character of Cambridge as a compact, dynamic city with a thriving historic core. When considering a potential development that would potentially extend the built edge of Cambridge significantly further than its current furthest extent, a score of Low is manifestly wrong.
- A similar point arises in relation to land parcel 8, which is awarded a High score for distance of the outer edge of the land parcel from the historic core but the overall score against the purpose is downgraded to Medium due to a Low score because the parcel does not lie adjacent to a main approach road.

5.1.22. In conclusion, the study purports to follow a *'robust and transparent'* methodology but fails to do so. It uses inappropriately small land parcels for assessment. The criteria and indicators used for the assessment omit many factors which are relevant to Green Belt purposes, and appear to be skewed towards understating the importance of South East Cambridge to Green Belt purposes. The principle of giving equal weight to all six of the purposes used in the assessments is inherently flawed. There are several anomalies in the detailed assessment of the land parcels in South East Cambridge. The

flawed nature of the methodology has been clearly demonstrated by its application to areas of land in South West and North East Cambridge, which yields results which are in direct conflict with the statement in the report that these areas are *'highly sensitive to change and important in respect of the Green Belt Purposes'*.

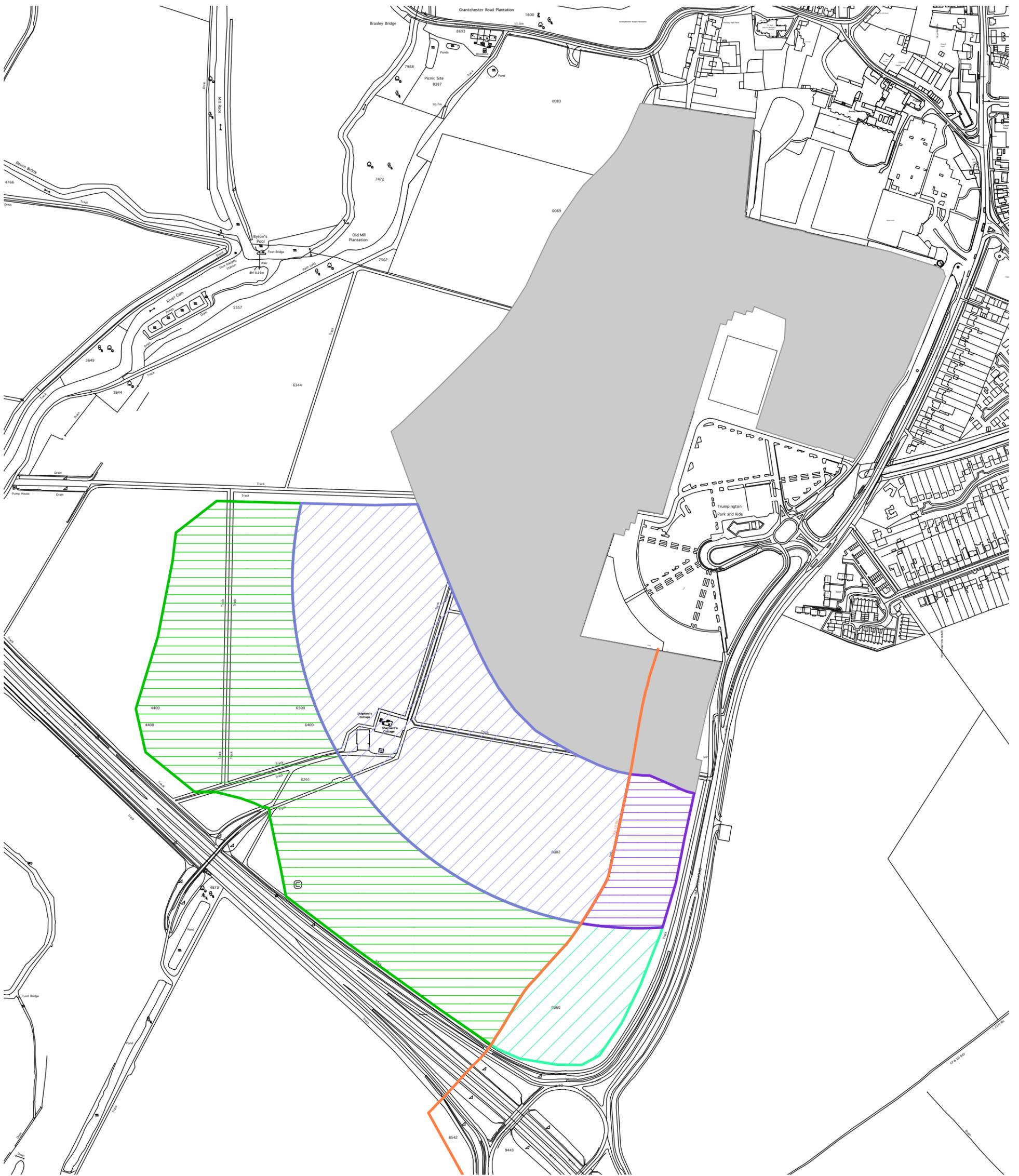
## 6.0 Quy Estate

- 6.1.1. This study relates to land at Fen Ditton, lying within sectors 18 and 19 as identified LDA Design's main report.
- 6.1.2. In section 1.2, the report states that the study follows a methodology developed by Liz Lake Associates in October 2012. The methodology is not explained in any detail in the report, although Appendix C contains 'Methodology Tables' for each parcel assessed in the study. The Methodology Tables set out National Green Belt purposes 1 to 4 and, for each purpose, set out criteria indicating substantial, moderate or slight adverse effects. For each land parcel, some of the criteria are highlighted, which appears to indicate the assessment made for that parcel.
- 6.1.3. With regard to the setting and special character of historic towns (National Green Belt purpose 4), the criteria in the Methodology Tables are inadequate for making any meaningful assessment of the performance of land in relation to this Green Belt purpose. No analysis or interpretation of 'setting' or 'special character' is given, so there is no indication as to the attributes a parcel of land should have if it is to contribute to this Green Belt Purpose. Thus, no explanation is provided as to how the assessment has been made.
- 6.1.4. At section 3.1, the report states that various studies were reviewed, including LDA Design's 2002 Study, but there is no evidence that the report has taken any account of the analysis or conclusions of that Study.
- 6.1.5. The Assessment of Green Belt Functions is included in section 6 and, as noted above, offers no explanation as to the assessments made, particularly in relation to National Green Belt purpose 4. Of the six parcels identified for assessment, three (WP1, EP1 and EP2) are assessed to play a *'limited role in the preservation of the local setting and special character of a historic town or significant feature of historic interest'*, with the other parcels assessed as playing no part in relation to setting or special character. The quoted wording differs from the wording used in the Methodology Tables in Appendix C, where the highlighted text for WP1 and EP1 indicates an assessment of Moderate. The assessment of EP2 in Appendix C as Slight is the same as other parcels assessed as playing no part in relation to setting or special character and is not reflected in the summary table in section 6.8.1, so there may be an error in the text in section 6.
- 6.1.6. These conclusions in relation to National Green Belt purpose 4 take no account of the role that all the parcels play in relation to the rural setting and identity of both Fen Ditton and Cambridge and the way in which the character and identity of the necklace villages contributes to the setting and special character of Cambridge.
- 6.1.7. Another inconsistency within the report arises in relation to parcel EP2. The text in section 6.5.1 in relation to National Green Belt purpose 1 states that it has Medium potential to lead to unrestricted sprawl, whereas the Methodology Table in Appendix C and the table in section 6.8.1 indicates Substantial.

- 6.1.8. The assessments are summarised in the table in section 6.8. The category ‘Substantial adverse effect’ in the Methodology Tables is reflected in Green in the table, indicating that a parcel is fulfilling the relevant Green Belt purpose. ‘Slight adverse effect’ is shown in red, indicating a parcel that is failing to meet the relevant Green Belt purpose.
- 6.1.9. The table includes a column headed ‘overall performance of the parcel to fulfil the functions of the Green Belt’, which appears to take an average of the performance against the five Green Belt purposes. This implies that Green Belt land should perform all Green Belt purposes but there is no indication in the NPPF that this should be the case.
- 6.1.10. All six parcels are given an overall assessment of Amber. Despite this, paragraph 7.1.6 states that three parcels are not considered of long term importance for the purposes of the Green Belt. The only explanation offered is that they have been *‘identified as parcels where initial development within Fen Ditton would be more suitable out of the six parcels assessed, due to their immediate proximity to the existing development boundary’*. This reason is not mentioned elsewhere in the report as a relevant criterion for assessment and there is nothing within the assessment that appears to justify these parcels as being more suitable for development than others.

**Review of Studies Submitted by Objectors**

**Appendix 1: Terence O'Rourke drawing number 173604/SK/600**



-  Cambridge City / South Cambs boundary
-  Trumpington Meadows development
-  Land to be removed from Green Belt in Cambridge City and allocated for a mixed use development as part of an extension to Trumpington Meadows comprising, residential use, indoor sport and ancillary supporting uses
-  Land to be removed from Green Belt in South Cambs and allocated for a mixed use development as part of an extension to Trumpington Meadows comprising, residential use, indoor sport and ancillary supporting uses
-  Area to remain as Green Belt within Cambridge City and allocated for outdoor sport at Trumpington
-  Area to remain as Green Belt within South Cambs and allocated for outdoor sport at Trumpington



**Review of Studies Submitted by Objectors**

**Appendix 2: LDA Design Assessment of Parcels of Land using CEG  
Methodology**

LDĀDESIGN

**Assessment of Land in South West Cambridge**

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## South West Cambridge Assessment

### Assessment Summary Table

| Cambridge Green Belt Purposes  | Scoring of Sub Areas against Green Belt Purposes |               |               |
|--|--|---------------|---------------|
|  | Sub Area 4.1                                     | Sub Area 4.2  | Sub Area 4.3  |
| Prevent communities in the environs of Cambridge from merging into one another and with the City         | <i>Medium</i>                                    | <i>Medium</i> | <i>Medium</i> |
| Ensure the protection of green corridors running from open countryside into the urban area               | <i>High</i>                                      | <i>Low</i>    | <i>Low</i>    |
| Assist in safeguarding the countryside from encroachment   | <i>High</i>                                      | <i>Medium</i> | <i>Medium</i> |
| To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre | <i>High</i>                                      | <i>High</i>   | <i>Medium</i> |
| To preserve the setting and special character of Cambridge   | <i>Low</i>                                       | <i>Medium</i> | <i>Low</i>    |
| Check unrestricted sprawl of large built up areas  | <i>Low</i>                                       | <i>Medium</i> | <i>Medium</i> |
| <b>Overall Contribution of Sub Area to Green Belt</b>  | <b>High</b>                                      | <b>Medium</b> | <b>Medium</b> |



Three empty rectangular boxes for notes or additional information.

South West Cambridge, Sub Area 4.1

| Criteria for Assessment   | Indicators used for Assessment  |   |
|---|---|---|
| <b>Urban Fringe or Gateway Development beyond the Built Edge</b>          | <ul style="list-style-type: none"> <li>None</li> </ul>  |   |
| <b>Relationship with Built Edge</b>                                       | <p>To the north, the parcel adjoins the built edge of the Newnham area of Cambridge at Grantchester Meadows Road. To the south, the parcel abuts the built edge of Grantchester necklace village. Both built edges are soft, green edges, with large mature trees, particularly the edge abutting Grantchester.</p> |   |
| <b>Rurality</b>   | <p>Landscape Features*</p> <ul style="list-style-type: none"> <li>Flood meadows</li> <li>Footpaths along river</li> <li>Well vegetated, with mature hedgerows and individual trees</li> </ul>   | <p>Townscape Features*</p> <ul style="list-style-type: none"> <li>1900-1945 Suburban Housing and Victorian/Edwardian Terraced Housing</li> <li>Bespoke houses of Grantchester with no regimented pattern and large gardens</li> </ul> |
| <p>Detractors:</p> <ul style="list-style-type: none"> <li>None</li> </ul> |   |   |

\*From Cambridge Landscape Character Assessment 2003, and Cambridge Green Belt Study 2002

South West Cambridge Sub Area 4.1: Assessment of Contribution to Green Belt Purposes

| Cambridge Green Belt Purposes  | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score                   |
|--|--|--|--|-------------------------|
| Prevent communities in the environs of Cambridge from merging into one another and with the City | Does the land parcel prevent communities in the environs of Cambridge from merging into one another and with the City? | Measured distance from the outer boundary of the land parcel and the nearest neighbouring urban area / settlement edge.  | Distance from south western corner of the sub area to the residential edge off Grantchester Meadows = 1.6km<br><br>Distance from south western corner of the sub area to the residential edge of Grantchester (High Street) = immediately adjacent | High                    |
|  |  | Analysis of topography and intervening vegetation to determine likely intervisibility of development of the land parcel and neighbouring settlement.                     | The sub area lies on low-lying flat land adjacent to the River Cam. Woodland associated with the River Cam and Grantchester will partially screen views to and from Grantchester and Trumpington (see <i>Assessment Sheet</i> )                    | Medium<br><b>Medium</b> |
| Ensure the protection of green corridors running from open countryside into the urban area       | Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?       | Assessment of whether the land parcel lies within a green corridor and the role it may play in facilitating connection between the urban area and the wider countryside. | The River Cam runs along the eastern boundary of the sub area, an area recognised as a green corridor ( <i>Cambridge Landscape Assessment, 2003</i> )  | <b>High</b>             |
| Assist in safeguarding the countryside from encroachment   | Does the land parcel create a soft green edge to the city, or have a distinctive urban edge?                           | Analysis of aerial imagery, photographs field work to establish relationship with built edge and degree of softening vegetation.   | Where the sub area abuts the urban edge, off Grantchester Meadows, woodland and mature trees soften the built edge and largely prevent visibility of buildings.  | High                    |

| Cambridge Green Belt Purposes   | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub Area to Green Belt Purposes   | Score          |
|---|---|---|---|----------------|
|   | Is the land parcel strongly rural in character?   | Assessment of the key characteristics and features of the land parcel, and identification of detractors within the local landscape that influence the perceived rurality and tranquillity of the land parcel.   | The sub area is characteristic of the River Cam Corridor Landscape Character Area ( <i>Cambridge Green Belt Study, 2002</i> ). There are limited detractors within or adjacent to the land parcel (see <i>Assessment Sheet</i> ).                       | High           |
|   | To what extent is the land parcel contained / separated from the wider countryside by landform?   | Analysis of topography and slope to determine whether land form contains the land parcel.   | The sub area is located on relatively flat low-lying land but views would be limited from the surrounding landscape by the vegetation along the river corridor and hedgerows/tree belts along field boundaries.   | Medium<br>High |
| <b>To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre</b> | To what extent would development of the land parcel increase the distance of the built edge from the historic centre?   | Comparison of distance of outer edge of land parcel from historic core with the existing extents of the built edge to the east of Cambridge   | The south of the sub area extends a greater distance from the historic core than the existing built edge and the gateway feature of the M11 junction with Barton Road and Coton Road (see <i>Urban Gateways and Distance from Historic Core plan</i> ). | High           |
|   | Would development of the land parcel extend the perceived urban gateways to the city* and lengthen the distance of the approaches to the historic core?<br><br>* As defined in the Cambridge Green Belt Study, 2002, dwg 07 | Identification of any existing gateway features along approach roads beyond the existing built edge, to include: <ul style="list-style-type: none"> <li>• Business and commercial premises;</li> <li>• Out of town retail and parking; and</li> <li>• Road signage, street lighting.</li> </ul> | The sub area lies adjacent to the River Cam and development of the sub area would therefore extend the perceived urban gateway (see <i>Urban Gateways and Distances from Historic Core plan</i> )   | High<br>High   |

| Cambridge Green Belt Purposes                                     | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score                |
|---|--|--|--|----------------------|
| <b>To preserve the setting and special character of Cambridge</b> | To what extent does the land parcel enable key or important views of the historic skyline of Cambridge?<br><br>Does the parcel form an open rural foreground in key or important views of the historic skyline of Cambridge? | Analysis of key views in order to establish the visibility of the historic skyline and whether the land parcel enables these.<br><br>Analysis of key views in order to establish whether the land parcel forms an open rural foreground to views of the historic core. | <b>Key Views 6, 9 and 10 (Grantchester Meadows):</b><br><br>There are intermittent views of landmark features within the historic core, such as the spire of the Roma Catholic Church and King’s College Chapel throughout this sub area, particularly from the locations of these key views. The sub area forms the foreground to these views and enables intermittent views of buildings within the Historic Core. | Medium               |
|   | Does the land parcel form part of a rural backdrop in views from within the historic core of the city?   | Analysis of views from Castle Mound within the historic core in order to establish whether the land parcel is visible and forms a backdrop to views across the city skyline.   | In views from within the city, the sub area is screened by intervening built form and does not form part of the backdrop to skyline views.   | Low<br><b>Low</b>    |
| <b>Check unrestricted sprawl of large built up areas</b>          | Does the land parcel assist in halting ribbon development?   | Identifying whether the land parcel lies along a route which may, if developed facilitate the linear / ribbon expansion of the city  | The sub area does not lie adjacent to a main route into Cambridge and therefore has limited potential to facilitate linear/ribbon expansion.   | Low                  |
|   | Is the land parcel closely associated with the existing built edge?  | Analysis of whether the land parcel adjoins the built edge and / or urban fringe development.  | To the north the sub area lies adjacent to the residential edge of Cambridge and to the south west it is adjacent to the residential edge of Grantchester, with open countryside to the east and west.   | Medium<br><b>Low</b> |
| <b>Overall Score</b>  |  |  |  | <b>High</b>          |



South West Cambridge, Sub Area 4.2

| Criteria for Assessment  | Indicators used for Assessment   |  |
|--|--|--|
| <b>Urban Fringe or Gateway Development beyond the Built Edge</b> | <ul style="list-style-type: none"> <li>Barton Road M11 junction</li> </ul>   |  |
| <b>Relationship with Built Edge</b>                              | The southern edge of the sub area is adjacent to the built edge of Grantchester to the south of the sub area, which is soft, green and well-treed. |  |
| <b>Rurality</b>  | Landscape Features* <ul style="list-style-type: none"> <li>Flat arable land</li> <li>Some treed hedgerow boundaries</li> </ul>                     | Townscape Features* <ul style="list-style-type: none"> <li>1900-1945 Suburban Housing and Victorian/Edwardian Terraced Housing beyond the Cambridge University sports grounds.</li> <li>Historic core and bespoke houses of Grantchester.</li> </ul> |
|  | Detractors: <ul style="list-style-type: none"> <li>Barton Road M11 junction</li> </ul>   |  |

\*From Cambridge Landscape Character Assessment 2003, and Cambridge Green Belt Study 2002

South West Cambridge Sub Area 4.2: Assessment of Contribution to Green Belt Purposes

| Cambridge Green Belt Purposes  | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes   | Score  |
|--|--|--|---|--------|
| Prevent communities in the environs of Cambridge from merging into one another and with the City | Does the land parcel prevent communities in the environs of Cambridge from merging into one another and with the City? | Measured distance from the outer boundary of the land parcel and the nearest neighbouring urban area / settlement edge.  | Distance from south western edge of the sub area to the residential edge off Fulbrooke Road = 1.4km<br><br>Distance from south western edge of the sub area to the residential edge of Grantchester (Coton Road) = immediately adjacent   | Medium |
|  |  | Analysis of topography and intervening vegetation to determine likely intervisibility of development of the land parcel and neighbouring settlement.                     | The sub area lies within relatively flat low-lying land west of Cambridge. Woodland and tree planting associated with the edge of Cambridge and Grantchester will partially screen views to and from Cambridge and Grantchester (see <i>Assessment Sheet</i> )                            | Medium |
| Ensure the protection of green corridors running from open countryside into the urban area       | Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?       | Assessment of whether the land parcel lies within a green corridor and the role it may play in facilitating connection between the urban area and the wider countryside. | The sub area does not lie within any area recognised as a green corridor ( <i>Cambridge Landscape Assessment, 2003</i> ) or serve to provide connection between a green corridor that penetrates the City and the wider countryside, nature conservation and recreation resources beyond. | Low    |
| Assist in safeguarding the countryside from encroachment   | Does the land parcel create a soft green edge to the city, or have a distinctive urban edge?                           | Analysis of aerial imagery, photographs field work to establish relationship with built edge and degree of softening vegetation.   | Mature trees and vegetation associated with garden and field boundaries north of Grantchester run along the southern boundary of the sub area and provide a degree of softening along the existing built edge.  | Medium |

| Cambridge Green Belt Purposes   | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score                   |
|---|---|---|--|-------------------------|
|   | Is the land parcel strongly rural in character?   | Assessment of the key characteristics and features of the land parcel, and identification of detractors within the local landscape that influence the perceived rurality and tranquillity of the land parcel.   | The sub area exhibits a number of the characteristics of the Rhee and Bourn Valleys Landscape Character Area ( <i>Cambridge Green Belt Study, 2002</i> ), including relatively flat landform and large arable fields. There are a small number of detractors adjacent to the land parcel, such as the Barton Road M11 junction (see <i>Assessment Sheet</i> ). | Medium                  |
|   | To what extent is the land parcel contained / separated from the wider countryside by landform?   | Analysis of topography and slope to determine whether land form contains the land parcel.   | The sub area is located on flat low-lying land but views would be possible from some areas in the surrounding landscape where vegetation along field boundaries is low and/or intermittent.  | Medium<br><b>Medium</b> |
| <b>To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre</b> | To what extent would development of the land parcel increase the distance of the built edge from the historic centre?   | Comparison of distance of outer edge of land parcel from historic core with the existing extents of the built edge to the east of Cambridge   | The south west of the sub area extends a greater distance from the historic core than the existing built edge and the gateway feature of the M11 junction with Barton Road and Coton Road (see <i>Urban Gateways and Distance from Historic Core plan</i> ).   | High                    |
|   | Would development of the land parcel extend the perceived urban gateways to the city* and lengthen the distance of the approaches to the historic core?<br><br>* As defined in the Cambridge Green Belt Study, 2002, dwg 07 | Identification of any existing gateway features along approach roads beyond the existing built edge, to include: <ul style="list-style-type: none"> <li>• Business and commercial premises;</li> <li>• Out of town retail and parking; and</li> <li>• Road signage, street lighting.</li> </ul> | The sub area lies adjacent to Barton Road and development of the sub area would therefore extend the perceived urban gateway (see <i>Urban Gateways and Distances from Historic Core plan</i> )  | High<br><b>High</b>     |

| Cambridge Green Belt Purposes  | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score       |
|--|---|---|--|-------------|
| <p><b>To preserve the setting and special character of Cambridge</b></p> | <p>To what extent does the land parcel enable key or important views of the historic skyline of Cambridge?</p> <p>Does the parcel form an open rural foreground in key or important views of the historic skyline of Cambridge?</p> | <p>Analysis of key views in order to establish the visibility of the historic skyline and whether the land parcel enables these.</p> <p>Analysis of key views in order to establish whether the land parcel forms an open rural foreground to views of the historic core.</p> | <p><b>Key View 2 (Coton Road):</b></p> <p>There are open views across this sub area towards the historic core, with many features prominent on the horizon, including the University Library, King’s College Chapel and the Roman Catholic Church spire. The sub area provides a rural foreground to this view.</p> <p><b>Key View 3 (Barton Road, crossing the M11 at junction 12):</b></p> <p>Vegetation along the M11 on slip largely prevents this sub area forming the foreground in views towards the Historic Core from this viewpoint.</p> <p><b>Key View 5 (Red Meadow Hill):</b></p> <p>There are open distant views towards the historic core with the University Library prominent on the skyline from this location. The sub area is visible from this viewpoint but does not form the foreground to the views of the historic core.</p> <p><b>Key View 7 (Barton Road):</b></p> <p>There are intermittent views through vegetation across this land parcel towards the historic core, with many features prominent on the horizon, including the University Library, King’s College Chapel and the Roman Catholic Church spire. The sub area provides a rural foreground to this view.</p> | <p>High</p> |

| Cambridge Green Belt Purposes                            | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score         |
|--|--|--|--|---------------|
|  | Does the land parcel form part of a rural backdrop in views from within the historic core of the city? | Analysis of views from Castle Mound within the historic core in order to establish whether the land parcel is visible and forms a backdrop to views across the city skyline. | In views from within the city, the sub area is screened by intervening built form and does not form part of the backdrop to skyline views. | Low           |
|  |  |  |  | <b>Medium</b> |
| <b>Check unrestricted sprawl of large built up areas</b> | Does the land parcel assist in halting ribbon development?   | Identifying whether the land parcel lies along a route which may, if developed facilitate the linear / ribbon expansion of the city  | The sub area lies adjacent to Barton Road, a main route into Cambridge, and therefore has potential to facilitate linear/ribbon expansion. | Medium        |
|  | Is the land parcel closely associated with the existing built edge?                                    | Analysis of whether the land parcel adjoins the built edge and / or urban fringe development.  | To the south the sub area lies adjacent to the residential edge of Grantchester, with open countryside in all other direction.             | Medium        |
| <b>Overall Score</b>                                     |  |  |  | <b>Medium</b> |



South West Cambridge, Sub Area 4.3

| Criteria for Assessment  | Indicators used for Assessment  |   |
|--|---|---|
| <b>Urban Fringe or Gateway Development beyond the Built Edge</b> | <ul style="list-style-type: none"> <li>Barton Road M11 junction</li> <li>Recreation Ground Sports Field buildings</li> <li>Model Railway – Cambridge Model Engineering Society</li> </ul> |   |
| <b>Relationship with Built Edge</b>                              | The sub area adjoins the built edge of Cambridge at Fulbrooke, Selwyn, and South Green Roads. The whole extent of the built edge is soft and well-treed.                                  |   |
| <b>Rurality</b>  | Landscape Features* <ul style="list-style-type: none"> <li>Tree belts and woodland</li> <li>Sports pitch</li> <li>Absence of arable fields</li> </ul>                                     | Townscape Features* <ul style="list-style-type: none"> <li>1900-1945 Suburban Housing and Victorian/Edwardian Terraced Housing</li> </ul> |
|  | Detractors: <ul style="list-style-type: none"> <li>Sports fields, including tennis courts (within parcel)</li> </ul>  |   |

\*From Cambridge Landscape Character Assessment 2003, and Cambridge Green Belt Study 2002

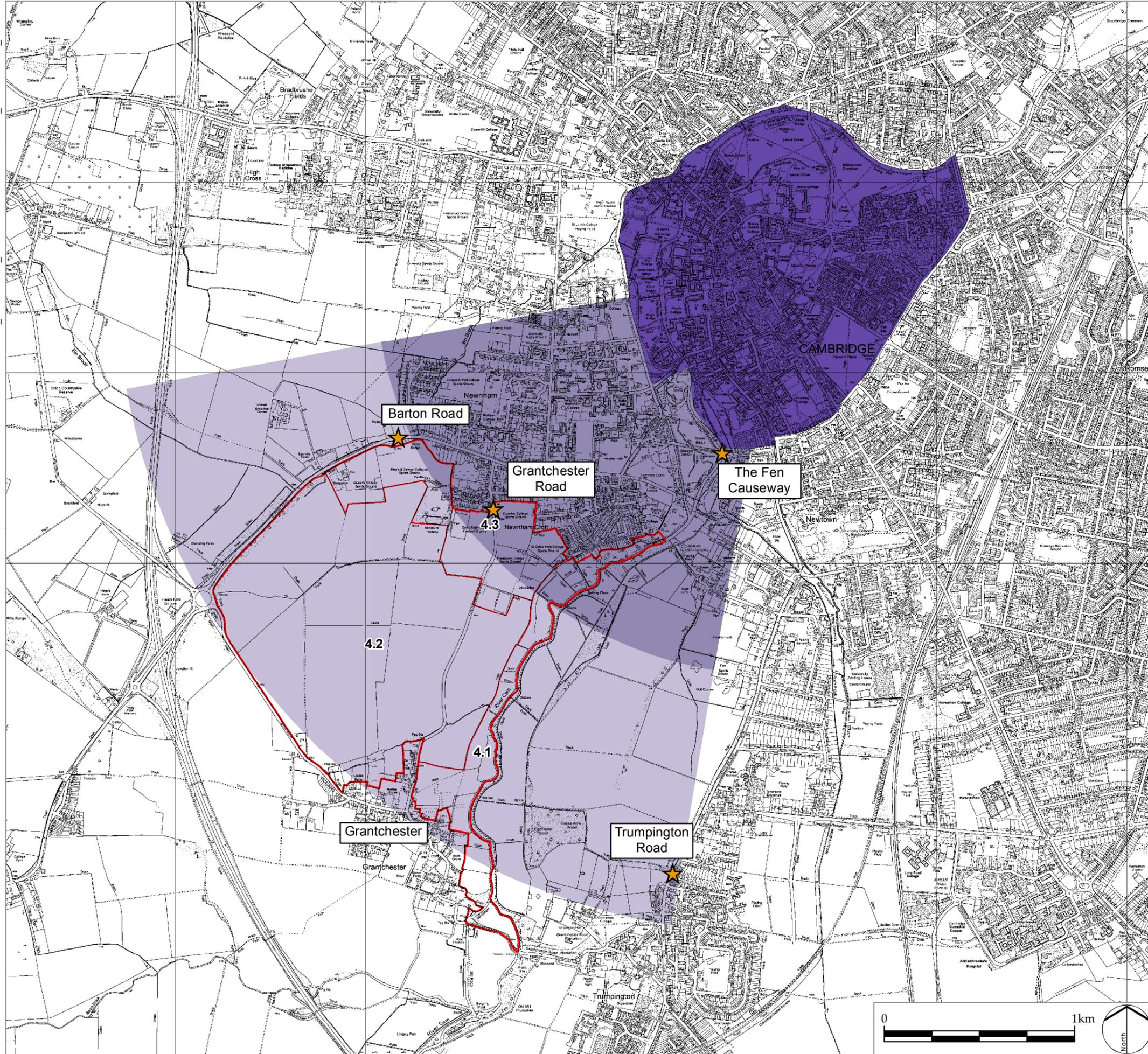
South West Cambridge Sub Area 4.3: Assessment of Contribution to Green Belt Purposes

| Cambridge Green Belt Purposes  | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes   | Score  |
|--|--|--|---|--------|
| Prevent communities in the environs of Cambridge from merging into one another and with the City | Does the land parcel prevent communities in the environs of Cambridge from merging into one another and with the City? | Measured distance from the outer boundary of the land parcel and the nearest neighbouring urban area / settlement edge.  | Distance from south western corner of the sub area to the residential edge off Fulbrooke Road = 550m<br><br>Distance from south western corner of the land parcel to the residential edge of Grantchester (Coton Road) = 1.4km  | Medium |
|  |  | Analysis of topography and intervening vegetation to determine likely intervisibility of development of the land parcel and neighbouring settlement.                     | The land parcel lies within relatively flat low-lying sub area of Cambridge. Woodland and tree planting associated with the edge of Cambridge and Grantchester will partially screen views to and from Cambridge and Grantchester (see <i>Assessment Sheet</i> )                          | Medium |
| Ensure the protection of green corridors running from open countryside into the urban area       | Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?       | Assessment of whether the land parcel lies within a green corridor and the role it may play in facilitating connection between the urban area and the wider countryside. | The sub area does not lie within any area recognised as a green corridor ( <i>Cambridge Landscape Assessment, 2003</i> ) or serve to provide connection between a green corridor that penetrates the City and the wider countryside, nature conservation and recreation resources beyond. | Low    |
| Assist in safeguarding the countryside from encroachment   | Does the land parcel create a soft green edge to the city, or have a distinctive urban edge?                           | Analysis of aerial imagery, photographs field work to establish relationship with built edge and degree of softening vegetation.   | Mature trees and vegetation associated with garden boundaries along the edge of Cambridge and around the sports pitches run along and through the northern boundary of the sub area and provide a degree of softening along the existing built edge.                                      | Medium |

| Cambridge Green Belt Purposes   | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score                   |
|---|---|---|--|-------------------------|
|   | Is the land parcel strongly rural in character?   | Assessment of the key characteristics and features of the land parcel, and identification of detractors within the local landscape that influence the perceived rurality and tranquillity of the land parcel.   | The sub area is not typical of the Rhee and Bourn Valleys Landscape Character Area ( <i>Cambridge Green Belt Study, 2002</i> ), due to the presence of large numbers of sports pitches and associated facilities. There are a number of detractors within the sub area, predominantly associated with the sports pitches (see <i>Assessment Sheet</i> ). | Low                     |
|   | To what extent is the land parcel contained / separated from the wider countryside by landform?   | Analysis of topography and slope to determine whether land form contains the land parcel.   | The sub area is located on relatively flat low-lying land but views would be possible from some areas in the surrounding landscape where vegetation along field boundaries is low and/or intermittent.   | Medium<br><b>Medium</b> |
| <b>To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre</b> | To what extent would development of the land parcel increase the distance of the built edge from the historic centre?   | Comparison of distance of outer edge of land parcel from historic core with the existing extents of the built edge to the east of Cambridge   | The south western edge of the sub area extends a greater distance from the historic core than the existing built edge, but is within the gateway feature of the M11 junction with Barton Road and Coton Road (see <i>Urban Gateways and Distance from Historic Core plan</i> ).  | Medium                  |
|   | Would development of the land parcel extend the perceived urban gateways to the city* and lengthen the distance of the approaches to the historic core?<br><br>* As defined in the Cambridge Green Belt Study, 2002, dwg 07 | Identification of any existing gateway features along approach roads beyond the existing built edge, to include: <ul style="list-style-type: none"> <li>• Business and commercial premises;</li> <li>• Out of town retail and parking; and</li> <li>• Road signage, street lighting.</li> </ul> | The sub area lies adjacent to Barton Road and development of the sub area would therefore extend the perceived urban gateway (see <i>Urban Gateways and Distances from Historic Core plan</i> )  | High<br><b>Medium</b>   |

| Cambridge Green Belt Purposes  | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub Area to Green Belt Purposes   | Score         |
|--|---|---|---|---------------|
| <p><b>To preserve the setting and special character of Cambridge</b></p> | <p>To what extent does the land parcel enable key or important views of the historic skyline of Cambridge?</p> <p>Does the parcel form an open rural foreground in key or important views of the historic skyline of Cambridge?</p> | <p>Analysis of key views in order to establish the visibility of the historic skyline and whether the land parcel enables these.</p> <p>Analysis of key views in order to establish whether the land parcel forms an open rural foreground to views of the historic core.</p> | <p><b>Key View 2 (Coton Road):</b></p> <p>There are open views towards the historic core, with many features prominent on the horizon, including the University Library, King’s College Chapel and the Roman Catholic Church spire. The sub area appears largely as part of the soft edge of Cambridge, contributing a small amount to a rural foreground to this view.</p> <p><b>Key View 3 (Barton Road, crossing the M11 at junction 12):</b></p> <p>Vegetation along the M11 on slip largely prevents this sub area forming the foreground in views towards the Historic Core from this viewpoint.</p> <p><b>Key View 5 (Red Meadow Hill):</b></p> <p>There are open distant views towards the historic core with the University Library prominent on the skyline from this location. The sub area is visible from this viewpoint but does not form the foreground to the views of the historic core.</p> <p><b>Key View 7 (Barton Road):</b></p> <p>There are intermittent views through vegetation towards the historic core, with many features prominent on the horizon, including the University Library, King’s College Chapel and the Roman Catholic Church spire. The sub area appears largely as part of the soft edge of Cambridge, contributing a small amount to a rural foreground to this view.</p> | <p>Medium</p> |

| Cambridge Green Belt Purposes                            | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score         |
|--|--|--|--|---------------|
|  | Does the land parcel form part of a rural backdrop in views from within the historic core of the city? | Analysis of views from Castle Mound within the historic core in order to establish whether the land parcel is visible and forms a backdrop to views across the city skyline. | In views from within the city, the sub area is screened by intervening built form and does not form part of the backdrop to skyline views. | Low           |
|  |  |  |  | <b>Low</b>    |
| <b>Check unrestricted sprawl of large built up areas</b> | Does the land parcel assist in halting ribbon development?   | Identifying whether the land parcel lies along a route which may, if developed facilitate the linear / ribbon expansion of the city  | The sub area lies adjacent to Barton Road, a main route into Cambridge, and therefore has potential to facilitate linear/ribbon expansion. | Medium        |
|  | Is the land parcel closely associated with the existing built edge?                                    | Analysis of whether the land parcel adjoins the built edge and / or urban fringe development.  | To the north east the sub area lies adjacent to the residential edge of Cambridge, with open countryside to the south.                     | Medium        |
| <b>Overall Score</b>                                     |  |  |  | <b>Medium</b> |



LEGEND

- Study Area
- Land Parcel Boundary (with reference number)
- Historic Core
- Extent of South West Cambridge
- Extent of Urban Gateways and Gateway Features beyond the Built Edge
- ★ Urban Gateways  
*(as identified in the Cambridge Green Belt Study, 2002 - Detailed Appraisal East of the City)*

# LDĀ DESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
**Figure 21:**  
South West Cambridge -  
Urban Gateways and Distance from Historic Core

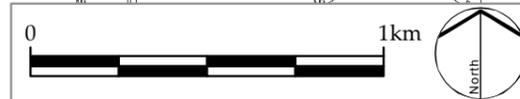
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| ISSUED BY | Peterborough  | T: 01733 310 471 |
| DATE      | November 2015 | DRAWN MPa        |
| SCALE @A3 | 1:20,000      | CHECKED RK       |
| STATUS    | Final         | APPROVED ChC     |

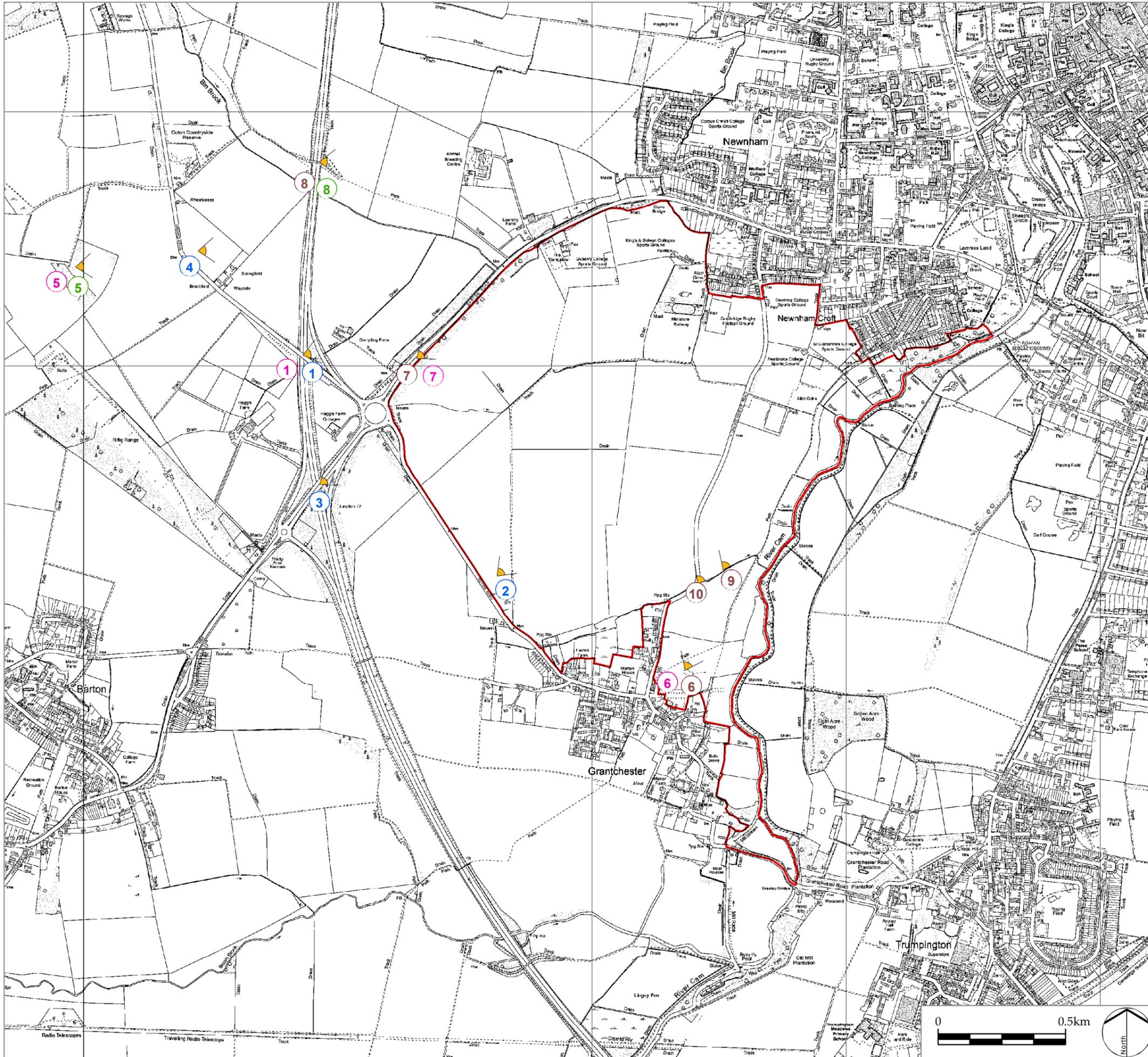
**DWG. NO. 4732\_021**

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Area measurements for indicative purposes only.

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Sources: Ordnance Survey





LEGEND

-  Study Area
-  Viewpoint

Cambridge Landscape Character Assessment, 2003 - Setting and Skyline Views

1. Grantchester Road, crossing the M11 at junction 12
2. Coton Road
3. Barton Road, crossing the M11 at junction 12
4. Grantchester Road, further north

Cambridge Skyline Guidance, 2012 - Strategic Viewpoints

1. Grantchester Road, crossing the M11 at junction 12
5. Red Meadow Hill, Coton Countryside Reserve
6. Grantchester Meadows
7. Barton Road, east of the junction 12 of the M11

Cambridge Green Belt Study, 2002 - Key Views and Gateways: First View of Cambridge

6. Grantchester Meadows - first view of Cambridge
7. Barton Road, east of the junction 12 of the M11 - key low-level view
8. Footbridge over M11, between junctions 12 and 13 - key low-level view
9. Grantchester Meadows (2) - key low-level view
10. Grantchester Meadows (3) - first view of Cambridge

2012 Inner Green Belt Boundary Study - Significant and Setting Views

5. Red Meadow Hill, Coton Countryside Reserve - significant view
8. Footbridge over M11, between junctions 12 and 13 - setting view

# LD A DESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
Figure 22:  
South West Cambridge -  
Key Views

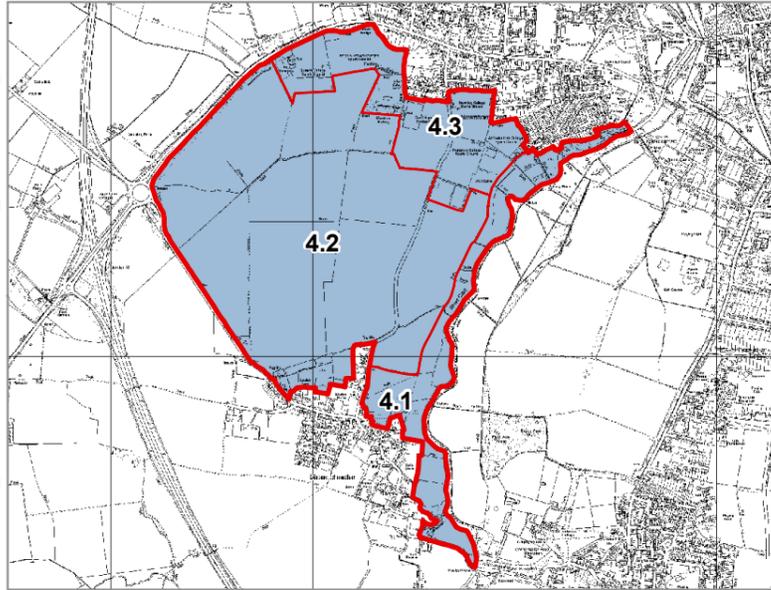
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| ISSUED BY | Peterborough  | T: 01733 310 471 |
| DATE      | November 2015 | DRAWN MPa        |
| SCALE @A3 | 1:15,000      | CHECKED RK       |
| STATUS    | Final         | APPROVED ChC     |

DWG. NO. 4732\_022

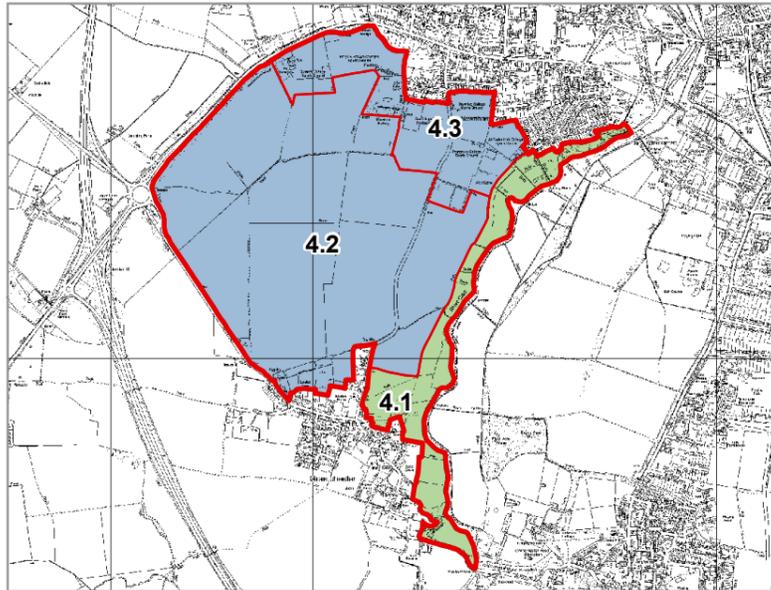
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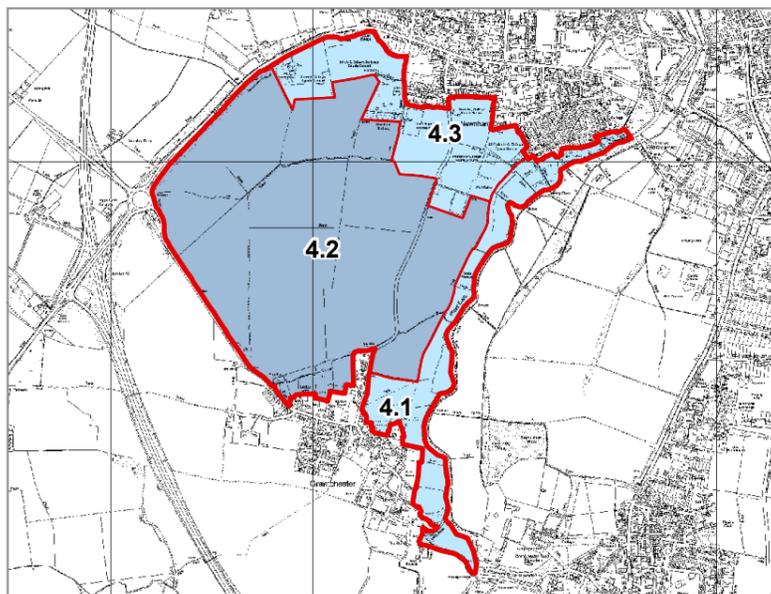
Sources: Ordnance Survey



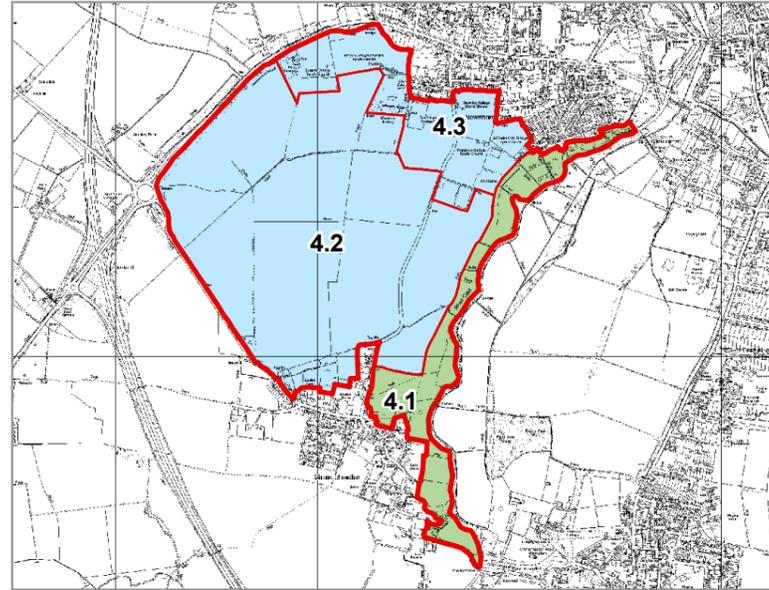
Prevent communities in the environs of Cambridge from merging into one another and with the city



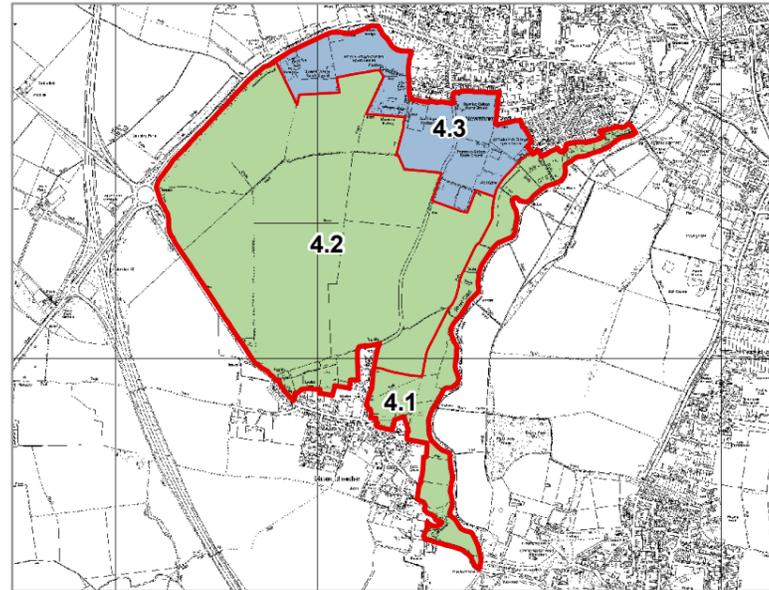
Assist in safeguarding countryside from encroachment



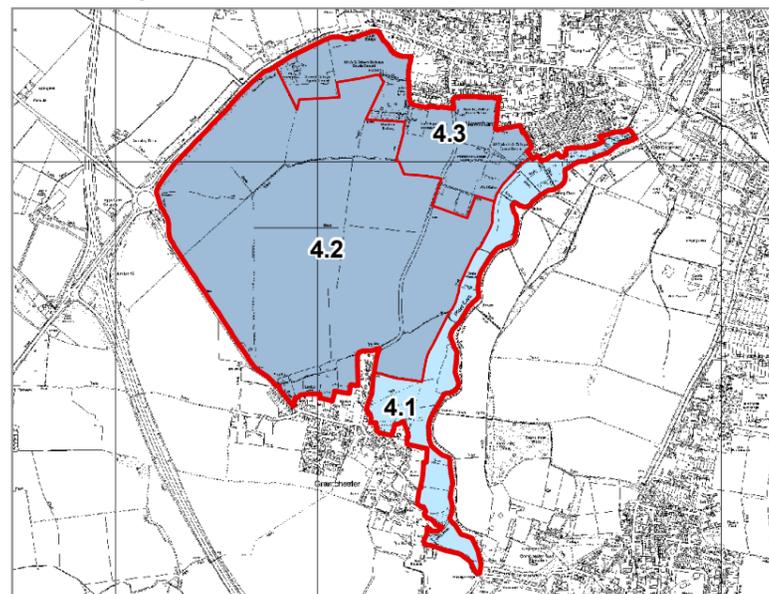
To preserve the setting and special character of Cambridge



Ensure the protection of green corridors running from open countryside into the urban area



To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre



Check unrestricted sprawl of large built up areas

LEGEND

- Study Area
- Land Parcel Boundary (with reference number)

Contribution of Land Parcel to Green Belt

- High
- Medium
- Low



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PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
Figure 23:  
South West Cambridge - Assessment Summary of  
Individual Green Belt Purposes

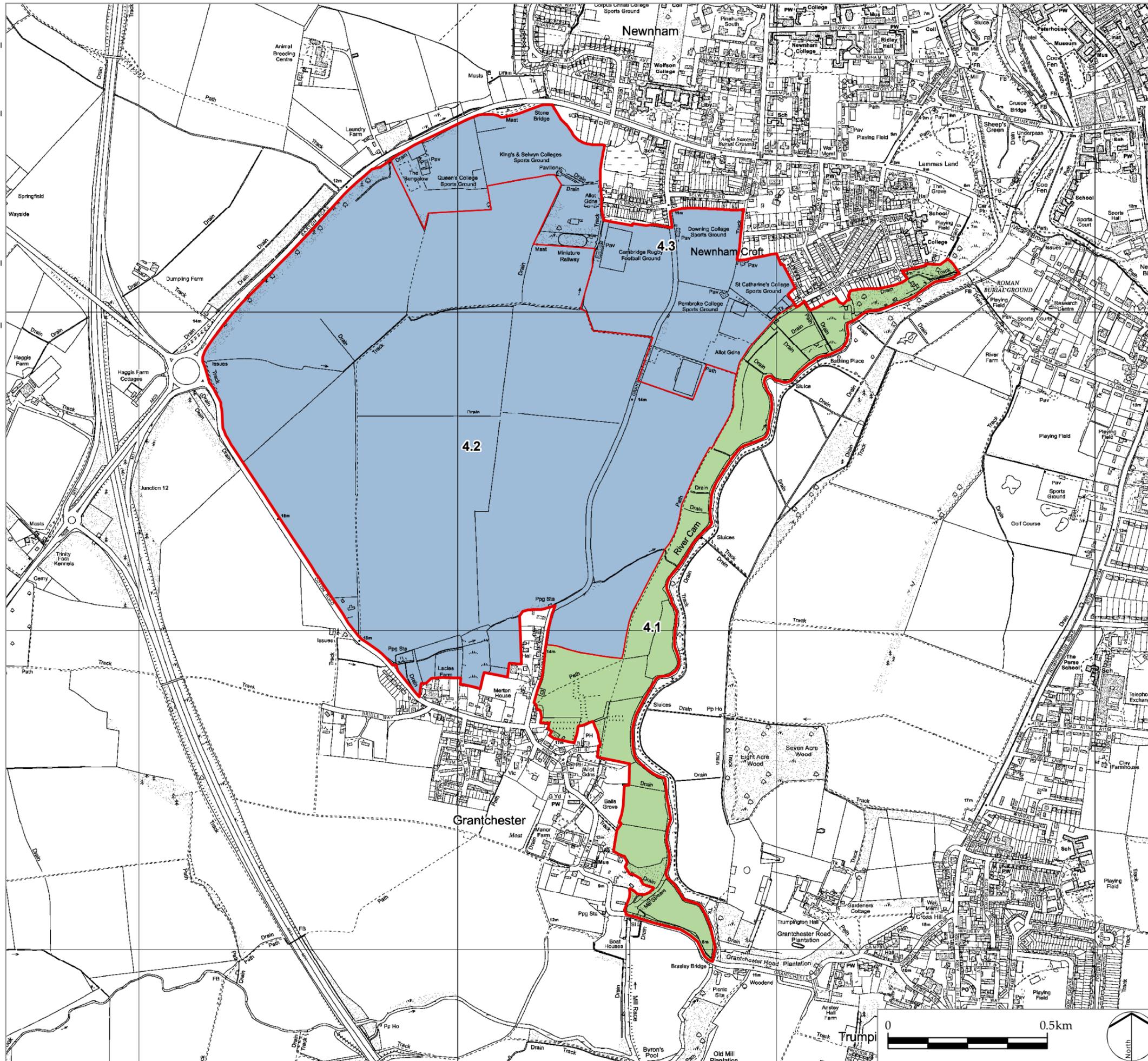
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|-----------|---------------|------------------|
| ISSUED BY | Peterborough  | T: 01733 310 471 |
| DATE      | November 2015 | DRAWN MPa        |
| SCALE @A3 | 1:35,000      | CHECKED RK       |
| STATUS    | Final         | APPROVED ChC     |

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Sources: Ordnance Survey



LEGEND

- Study Area
- Land Parcel Boundary (with reference number)

Contribution of Land Parcel to Green Belt

- High
- Medium
- Low

# LDĀDESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
**Figure 24:**  
South West Cambridge - Assessment Summary

|           |               |                  |
|-----------|---------------|------------------|
| ISSUED BY | Peterborough  | T: 01733 310 471 |
| DATE      | November 2015 | DRAWN MPa        |
| SCALE @A3 | 1:12,000      | CHECKED RK       |
| STATUS    | Final         | APPROVED ChC     |

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LDĀDESIGN

**Assessment of Land in North East Cambridge**

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## North East Cambridge Assessment

### Assessment Summary Table

| Cambridge Green Belt Purposes  | Scoring of Sub Areas against Green Belt Purposes |               |               |
|--|--|---------------|---------------|
|  | Sub Area 18.1                                    | Sub Area 18.2 | Sub Area 19.1 |
| Prevent communities in the environs of Cambridge from merging into one another and with the City         | <i>High</i>                                      | <i>Medium</i> | <i>Medium</i> |
| Ensure the protection of green corridors running from open countryside into the urban area               | <i>Low</i>                                       | <i>Low</i>    | <i>Low</i>    |
| Assist in safeguarding the countryside from encroachment   | <i>Medium</i>                                    | <i>Medium</i> | <i>Medium</i> |
| To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre | <i>Medium</i>                                    | <i>Low</i>    | <i>Low</i>    |
| To preserve the setting and special character of Cambridge   | <i>Low</i>                                       | <i>Low</i>    | <i>Low</i>    |
| Check unrestricted sprawl of large built up areas  | <i>Medium</i>                                    | <i>Low</i>    | <i>Low</i>    |
| <b>Overall Contribution of Sub Area to Green Belt</b>  | <b>Medium</b>                                    | <b>Low</b>    | <b>Low</b>    |



North East Cambridge, Sub Area 18.1

| Criteria for Assessment  | Indicators used for Assessment  |   |
|--|---|---|
| <b>Urban Fringe or Gateway Development beyond the Built Edge</b>   | <ul style="list-style-type: none"> <li>Residential properties within the eastern part of the parcel</li> <li>A14 junction with Horningsea Road</li> </ul>   |   |
| <b>Relationship with Built Edge</b>  | <p>The sub area is bordered by the built edge of Fen Ditton along High Ditch Road to the north and the properties of Ditton Lane to the east. The housing along Fison Road and Thorpe Road on the north eastern edge of Cambridge are obscured and separated from the parcel by the well vegetated disused railway line that forms the southern boundary.</p> |   |
| <b>Rurality</b>  | <b>Landscape Features*</b> <ul style="list-style-type: none"> <li>Small field parcels bordered by mature treed hedgerows</li> <li>Well vegetated disused railway line</li> </ul>  | <b>Townscape Features*</b> <ul style="list-style-type: none"> <li>Post-war suburban housing of East Cambridge to the south.</li> <li>Residential properties with large gardens in Fen Ditton</li> </ul> |
| <b>Detractors:</b> <ul style="list-style-type: none"> <li>Disused and run down farm buildings in the east of the sub area</li> </ul> |   |   |

\*From Cambridge Landscape Character Assessment 2003, and Cambridge Green Belt Study 2002

North East Cambridge Sub Area 18.1: Assessment of Contribution to Green Belt Purposes

| Cambridge Green Belt Purposes  | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score |
|--|--|--|--|-------|
| Prevent communities in the environs of Cambridge from merging into one another and with the City | Does the land parcel prevent communities in the environs of Cambridge from merging into one another and with the City? | Measured distance from the outer boundary of the land parcel and the nearest neighbouring urban area / settlement edge.  | Distance from northern corner of the sub area to the residential edge off Fison Road = 220m<br><br>Distance from northern corner of the sub area to the residential edge of Fen Ditton (High Ditch Road) = immediately adjacent  | High  |
|  |  | Analysis of topography and intervening vegetation to determine likely intervisibility of development of the land parcel and neighbouring settlement.                     | The sub area lies within relatively flat low-lying land north east of Cambridge. Woodland and tree planting associated with the disused railway line along the edge of Cambridge and field boundaries south of Fen Ditton will partially screen views to and from Cambridge and Fen Ditton, but there will be clear visibility into the sub area from some properties in Fen Ditton (see <i>Assessment Sheet</i> ) | High  |
| Ensure the protection of green corridors running from open countryside into the urban area       | Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?       | Assessment of whether the land parcel lies within a green corridor and the role it may play in facilitating connection between the urban area and the wider countryside. | The sub area does not lie within any area recognised as a green corridor ( <i>Cambridge Landscape Assessment, 2003</i> ) or serve to provide connection between a green corridor that penetrates the City and the wider countryside, nature conservation and recreation resources beyond.  | Low   |
| Assist in safeguarding the countryside from encroachment   | Does the land parcel create a soft green edge to the city, or have a distinctive urban edge?                           | Analysis of aerial imagery, photographs field work to establish relationship with built edge and degree of softening vegetation.   | Where the sub area abuts the urban edge, off Fison Road, woodland and mature trees along the disused railway line soften the built edge and largely prevent visibility of buildings.   | High  |

| Cambridge Green Belt Purposes   | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score            |
|---|---|---|--|------------------|
|   | Is the land parcel strongly rural in character?   | Assessment of the key characteristics and features of the land parcel, and identification of detractors within the local landscape that influence the perceived rurality and tranquillity of the land parcel.   | The sub area is not typical of the Eastern Fen Edge Landscape Character Area ( <i>Cambridge Green Belt Study, 2002</i> ), due to the small scale and enclosed nature of the sub area. There are a number of disused and run down farm buildings within the east of this sub area that are localised detractors within the sub area (see <i>Assessment Sheet</i> ). | Low              |
|   | To what extent is the land parcel contained / separated from the wider countryside by landform?   | Analysis of topography and slope to determine whether land form contains the land parcel.   | The sub area is located on relatively flat low-lying land but views would be possible of parts of the sub area from the surrounding landscape, where vegetation along field boundaries is low and/or intermittent.   | Medium<br>Medium |
| <b>To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre</b> | To what extent would development of the land parcel increase the distance of the built edge from the historic centre?   | Comparison of distance of outer edge of land parcel from historic core with the existing extents of the built edge to the east of Cambridge   | The eastern edge of the sub area extends a greater distance from the historic core than the existing built edge, but within the gateway feature of the A14 junction with Horningsea Road (see <i>Urban Gateways and Distance from Historic Core plan</i> ).  | Medium           |
|   | Would development of the land parcel extend the perceived urban gateways to the city* and lengthen the distance of the approaches to the historic core?<br><br>* As defined in the Cambridge Green Belt Study, 2002, dwg 07 | Identification of any existing gateway features along approach roads beyond the existing built edge, to include: <ul style="list-style-type: none"> <li>• Business and commercial premises;</li> <li>• Out of town retail and parking; and</li> <li>• Road signage, street lighting.</li> </ul> | The sub area lies adjacent to Ditton Lane/B1047 and development of the sub area would therefore extend the perceived urban gateway (see <i>Urban Gateways and Distances from Historic Core plan</i> )  | High<br>Medium   |

| Cambridge Green Belt Purposes                                     | Assessment Criteria   | Assessment Indicator   | Assessment of Contribution of Sub Area to Green Belt Purposes  | Score         |
|---|---|--|--|---------------|
| <b>To preserve the setting and special character of Cambridge</b> | To what extent does the land parcel enable key or important views of the historic skyline of Cambridge?       | Analysis of key views in order to establish the visibility of the historic skyline and whether the land parcel enables these.  | The sub area does not enable views of the historic core from the Key Views 1, 2 or 3.  | Low           |
|   | Does the parcel form an open rural foreground in key or important views of the historic skyline of Cambridge? | Analysis of key views in order to establish whether the land parcel forms an open rural foreground to views of the historic core.  |  |               |
|   | Does the land parcel form part of a rural backdrop in views from within the historic core of the city?        | Analysis of views from Castle Mound within the historic core in order to establish whether the land parcel is visible and forms a backdrop to views across the city skyline. | In views from within the city, the sub area is screened by intervening built form and does not form part of the backdrop to skyline views. | Low           |
| <b>Check unrestricted sprawl of large built up areas</b>          | Does the land parcel assist in halting ribbon development?  | Identifying whether the land parcel lies along a route which may, if developed facilitate the linear / ribbon expansion of the city  | The sub area lies adjacent to the B1047, a main route into Cambridge, and therefore has potential to facilitate linear/ribbon expansion.   | High          |
|   | Is the land parcel closely associated with the existing built edge?   | Analysis of whether the land parcel adjoins the built edge and / or urban fringe development.  | The sub area lies between the existing built edge of Cambridge and the built edge of Fen Ditton.   | Low           |
| <b>Overall Score</b>  |   |  |  | <b>Medium</b> |



North East Cambridge, Sub Area 18.2

| Criteria for Assessment  | Indicators used for Assessment   |  |
|--|--|--|
| <b>Urban Fringe or Gateway Development beyond the Built Edge</b> | <ul style="list-style-type: none"> <li>Residential properties along Horningsea Road</li> <li>A14 junction with Horningsea Road</li> </ul>  |  |
| <b>Relationship with Built Edge</b>                              | <p>This sub area adjoins the built edge of housing in Fen Ditton along High Ditch Road to the south and Horningsea Road to the west. The older part of the housing that forms the western part of the built edge is well treed, the built edge of the newer housing is more stark.</p> |  |
| <b>Rurality</b>  | <b>Landscape Features*</b> <ul style="list-style-type: none"> <li>Large flat arable fields</li> <li>Few Hedgerows</li> <li>Well vegetated disused railway line</li> </ul>  | <b>Townscape Features*</b> <ul style="list-style-type: none"> <li>Residential cul-de-sac of Musgrave Way (within parcel)</li> <li>Bespoke houses and 21<sup>st</sup> century housing of Fen Ditton along High Ditch Road to the south</li> </ul> |
|  | <b>Detractors:</b> <ul style="list-style-type: none"> <li>A14 to the north</li> </ul>  |  |

\*From Cambridge Landscape Character Assessment 2003, and Cambridge Green Belt Study 2002

North East Cambridge Sub Area 18.2: Assessment of Contribution to Green Belt Purposes

| Cambridge Green Belt Purposes  | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub area to Green Belt Purposes   | Score  |
|--|--|--|---|--------|
| Prevent communities in the environs of Cambridge from merging into one another and with the City | Does the land parcel prevent communities in the environs of Cambridge from merging into one another and with the City? | Measured distance from the outer boundary of the land parcel and the nearest neighbouring urban area / settlement edge.  | Distance from northern corner of the sub area to the residential edge off Fison Road = 1.3km<br><br>Distance from northern corner of the sub area to the residential edge of Fen Ditton (Horningsea Road) = 400m  | Medium |
|  |  | Analysis of topography and intervening vegetation to determine likely intervisibility of development of the land parcel and neighbouring settlement.                     | The sub area lies within relatively flat low-lying land north east of Cambridge. There would be clear visibility into the sub area from some properties in Fen Ditton (see <i>Assessment Sheet</i> )  | High   |
| Ensure the protection of green corridors running from open countryside into the urban area       | Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?       | Assessment of whether the land parcel lies within a green corridor and the role it may play in facilitating connection between the urban area and the wider countryside. | The sub area does not lie within any area recognised as a green corridor ( <i>Cambridge Landscape Assessment, 2003</i> ) or serve to provide connection between a green corridor that penetrates the City and the wider countryside, nature conservation and recreation resources beyond. | Low    |
| Assist in safeguarding the countryside from encroachment   | Does the land parcel create a soft green edge to the city, or have a distinctive urban edge?                           | Analysis of aerial imagery, photographs field work to establish relationship with built edge and degree of softening vegetation.   | Parts of the built edge of Fen Ditton adjacent to this sub area have limited vegetation and do not have a soft green edge.  | Low    |

| Cambridge Green Belt Purposes   | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub area to Green Belt Purposes   | Score  |
|---|---|---|---|--------|
|   | Is the land parcel strongly rural in character?   | Assessment of the key characteristics and features of the land parcel, and identification of detractors within the local landscape that influence the perceived rurality and tranquillity of the land parcel.   | The sub area exhibits a number of the characteristics of the Eastern Fen Edge Landscape Character Area ( <i>Cambridge Green Belt Study, 2002</i> ), being relatively open arable land with occasional hawthorn hedgerows. The A14 runs along the north eastern boundary of the sub area and is a visual and audible detractor (see <i>Assessment Sheet</i> ). | Medium |
|   | To what extent is the land parcel contained / separated from the wider countryside by landform?   | Analysis of topography and slope to determine whether land form contains the land parcel.   | The sub area is located on flat low-lying land but views would be possible from some areas in the surrounding landscape where vegetation along field boundaries is low and/or intermittent.   | Medium |
| <b>To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre</b> | To what extent would development of the land parcel increase the distance of the built edge from the historic centre?   | Comparison of distance of outer edge of land parcel from historic core with the existing extents of the built edge to the east of Cambridge   | The north east of the sub area extends a greater distance from the historic core than the existing built edge, but within the gateway feature of the A14 junction with Horningsea Road (see <i>Urban Gateways and Distance from Historic Core plan</i> ).   | Medium |
|   | Would development of the land parcel extend the perceived urban gateways to the city* and lengthen the distance of the approaches to the historic core?<br><br>* As defined in the Cambridge Green Belt Study, 2002, dwg 07 | Identification of any existing gateway features along approach roads beyond the existing built edge, to include: <ul style="list-style-type: none"> <li>• Business and commercial premises;</li> <li>• Out of town retail and parking; and</li> <li>• Road signage, street lighting.</li> </ul> | The sub area lies adjacent to the B1047, which is an approach road to Fen Ditton and then Cambridge, but is beyond Fen Ditton and development of the sub area would therefore not extend the perceived urban gateway (see <i>Urban Gateways and Distance from Historic Core plan</i> ).   | Low    |
|   |   |   |   | Low    |

| Cambridge Green Belt Purposes                                     | Assessment Criteria   | Assessment Indicator   | Assessment of Contribution of Sub area to Green Belt Purposes  | Score      |
|---|---|--|--|------------|
| <b>To preserve the setting and special character of Cambridge</b> | To what extent does the land parcel enable key or important views of the historic skyline of Cambridge?       | Analysis of key views in order to establish the visibility of the historic skyline and whether the land parcel enables these.  | The sub area does not enable views of the historic core from the Key Views 1, 2 or 3.  | Low        |
|   | Does the parcel form an open rural foreground in key or important views of the historic skyline of Cambridge? | Analysis of key views in order to establish whether the land parcel forms an open rural foreground to views of the historic core.  |  |            |
|   | Does the land parcel form part of a rural backdrop in views from within the historic core of the city?        | Analysis of views from Castle Mound within the historic core in order to establish whether the land parcel is visible and forms a backdrop to views across the city skyline. | In views from within the city, the sub area is screened by intervening built form and does not form part of the backdrop to skyline views.                           | Low        |
| <b>Check unrestricted sprawl of large built up areas</b>          | Does the land parcel assist in halting ribbon development?  | Identifying whether the land parcel lies along a route which may, if developed facilitate the linear / ribbon expansion of the city  | The sub area does not lie adjacent to an identified main route into Cambridge and therefore has limited potential to facilitate linear/ribbon expansion.             | Low        |
|   | Is the land parcel closely associated with the existing built edge?   | Analysis of whether the land parcel adjoins the built edge and / or urban fringe development.  | The sub area lies adjacent to the built edge of Fen Ditton to the south west and west, and fronts open countryside to the east and beyond the A14 to the north east. | Medium     |
| <b>Overall Score</b>  |   |  |  | <b>Low</b> |



North East Cambridge, Sub Area 19.1

| Criteria for Assessment  | Indicators used for Assessment   |  |
|--|--|--|
| <b>Urban Fringe or Gateway Development beyond the Built Edge</b> | <ul style="list-style-type: none"> <li>Residential properties along Horningsea Road</li> <li>A14 junction with Horningsea Road</li> </ul>  |  |
| <b>Relationship with Built Edge</b>                              | <p>This parcel is partially bordered by built edges of Fen Ditton on the western, southern and eastern boundaries. The built edges comprise the bespoke residential properties along Green End Road, Stanbury Close and Fen Ditton High Street. All the built edges are soft and well-treed.</p> |  |
| <b>Rurality</b>  | Landscape Features* <ul style="list-style-type: none"> <li>Large arable fields with treed hedgerow boundaries</li> </ul>   | Townscape Features* <ul style="list-style-type: none"> <li>Bespoke houses and historic core of Fen Ditton</li> </ul> |
|  | Detractors: <ul style="list-style-type: none"> <li>A14 to the north</li> </ul>   |  |

\*From Cambridge Landscape Character Assessment 2003, and Cambridge Green Belt Study 2002

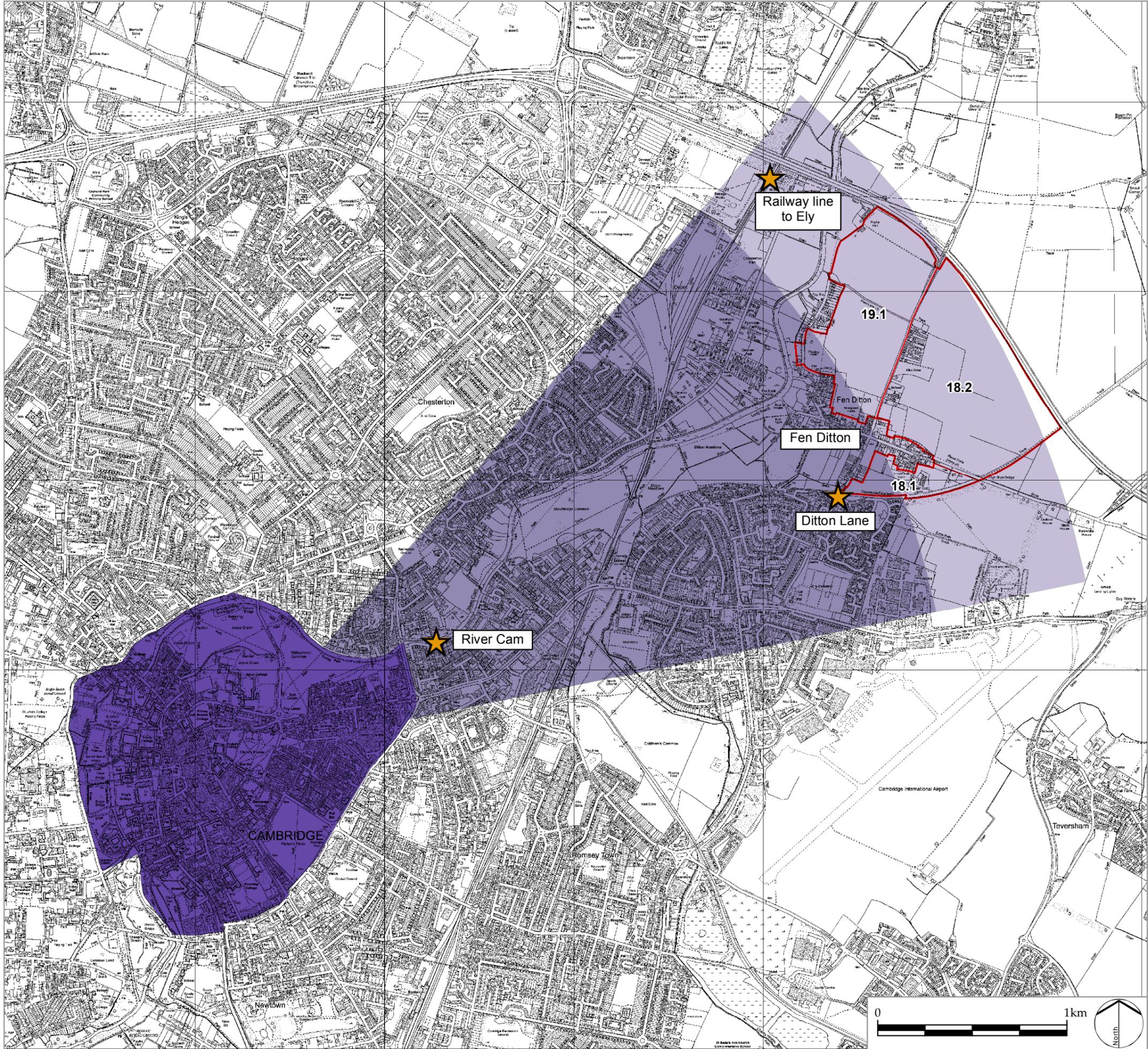
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North East Cambridge Sub Area 19.1: Assessment of Contribution to Green Belt Purposes

| Cambridge Green Belt Purposes  | Assessment Criteria  | Assessment Indicator   | Assessment of Contribution of Sub area to Green Belt Purposes   | Score              |
|--|--|--|---|--------------------|
| Prevent communities in the environs of Cambridge from merging into one another and with the City | Does the land parcel prevent communities in the environs of Cambridge from merging into one another and with the City? | Measured distance from the outer boundary of the land parcel and the nearest neighbouring urban area / settlement edge.  | Distance from northern corner of the sub area to the residential edge off Dunsmore Close = 1.5km<br><br>Distance from northern corner of the sub area to the residential edge of Fen Ditton (Green End) = 450m  | Medium             |
|  |  | Analysis of topography and intervening vegetation to determine likely intervisibility of development of the land parcel and neighbouring settlement.                     | The sub area lies within relatively flat low-lying land north east of Cambridge and close to the River Cam corridor. There would be clear visibility into the sub area from some properties in Fen Ditton (see <i>Assessment Sheet</i> )  | High<br><br>Medium |
| Ensure the protection of green corridors running from open countryside into the urban area       | Does the land parcel lie within a green corridor penetrating the city and connecting with the wider countryside?       | Assessment of whether the land parcel lies within a green corridor and the role it may play in facilitating connection between the urban area and the wider countryside. | The sub area does not lie within any area recognised as a green corridor ( <i>Cambridge Landscape Assessment, 2003</i> ) or serve to provide connection between a green corridor that penetrates the City and the wider countryside, nature conservation and recreation resources beyond. | Low                |
| Assist in safeguarding the countryside from encroachment   | Does the land parcel create a soft green edge to the city, or have a distinctive urban edge?                           | Analysis of aerial imagery, photographs field work to establish relationship with built edge and degree of softening vegetation.   | Mature trees and vegetation associated with garden and field boundaries along the edges of Fen Ditton run along the southern and western boundaries of the sub area and provide a degree of softening along the existing built edge.  | Medium             |

| Cambridge Green Belt Purposes   | Assessment Criteria   | Assessment Indicator  | Assessment of Contribution of Sub area to Green Belt Purposes  | Score  |
|---|---|---|--|--------|
|   | Is the land parcel strongly rural in character?   | Assessment of the key characteristics and features of the land parcel, and identification of detractors within the local landscape that influence the perceived rurality and tranquillity of the land parcel.   | The sub area exhibits a number of the characteristics of the Eastern Fen Edge Landscape Character Area ( <i>Cambridge Green Belt Study, 2002</i> ), being relatively open arable land with occasional hawthorn hedgerows. The A14 runs along the northern boundary of the sub area and is an audible detractor (see <i>Assessment Sheet</i> ). | Medium |
|   | To what extent is the land parcel contained / separated from the wider countryside by landform?   | Analysis of topography and slope to determine whether land form contains the land parcel.   | The sub area is located on flat low-lying land but views would be possible from areas in the surrounding landscape, particularly along Horningsea Road where there is little roadside vegetation.  | Medium |
| <b>To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre</b> | To what extent would development of the land parcel increase the distance of the built edge from the historic centre?   | Comparison of distance of outer edge of land parcel from historic core with the existing extents of the built edge to the east of Cambridge   | The north of the sub area extends a greater distance from the historic core than the existing built edge, but within the gateway feature of the A14 junction with Horningsea Road (see <i>Urban Gateways and Distance from Historic Core plan</i> ).   | Medium |
|   | Would development of the land parcel extend the perceived urban gateways to the city* and lengthen the distance of the approaches to the historic core?<br><br>* As defined in the Cambridge Green Belt Study, 2002, dwg 07 | Identification of any existing gateway features along approach roads beyond the existing built edge, to include: <ul style="list-style-type: none"> <li>• Business and commercial premises;</li> <li>• Out of town retail and parking; and</li> <li>• Road signage, street lighting.</li> </ul> | The sub area lies adjacent to the B1047, which is an approach road to Fen Ditton and then Cambridge, but is beyond Fen Ditton and development of the sub area would therefore not extend the perceived urban gateway (see <i>Urban Gateways and Distance from Historic Core plan</i> ).  | Low    |
|   |   |   |  | Low    |

| Cambridge Green Belt Purposes                                     | Assessment Criteria   | Assessment Indicator   | Assessment of Contribution of Sub area to Green Belt Purposes  | Score      |
|---|---|--|--|------------|
| <b>To preserve the setting and special character of Cambridge</b> | To what extent does the land parcel enable key or important views of the historic skyline of Cambridge?       | Analysis of key views in order to establish the visibility of the historic skyline and whether the land parcel enables these.  | Key View 3 has partial views across this sector towards the historic skyline, which continue in places when travelling south along the B1047 towards Fen Ditton. The sub area forms the foreground in views towards Fen Ditton and consequently towards the historic skyline, although these views are not as obvious as they would have been in 2002 as a consequence of vegetation growth since that time. | Medium     |
|   | Does the parcel form an open rural foreground in key or important views of the historic skyline of Cambridge? | Analysis of key views in order to establish whether the land parcel forms an open rural foreground to views of the historic core.  |  |            |
|   | Does the land parcel form part of a rural backdrop in views from within the historic core of the city?        | Analysis of views from Castle Mound within the historic core in order to establish whether the land parcel is visible and forms a backdrop to views across the city skyline. | In views from within the city, the sub area is screened by intervening built form and does not form part of the backdrop to skyline views.   | Low        |
| <b>Check unrestricted sprawl of large built up areas</b>          | Does the land parcel assist in halting ribbon development?  | Identifying whether the land parcel lies along a route which may, if developed facilitate the linear / ribbon expansion of the city  | The sub area does not lie adjacent to an identified main route into Cambridge and therefore has limited potential to facilitate linear/ribbon expansion.   | Low        |
|   | Is the land parcel closely associated with the existing built edge?   | Analysis of whether the land parcel adjoins the built edge and / or urban fringe development.  | The sub area lies adjacent to the built edge of Fen Ditton to the south and west, and fronts open countryside to the north west and beyond the A14 to the north east.  | Medium     |
| <b>Overall Score</b>  |   |  |  | <b>Low</b> |



LEGEND

- Study Area
- Land Parcel Boundary (with reference number)
- Historic Core
- Extent of North East Cambridge
- Extent of Urban Gateways and Gateway Features beyond the Built Edge
- Urban Gateways  
*(as identified in the Cambridge Green Belt Study, 2002 - Detailed Appraisal East of the City)*

# LDĀ DESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
**Figure 25:**  
North East Cambridge -  
Urban Gateways and Distance from Historic Core

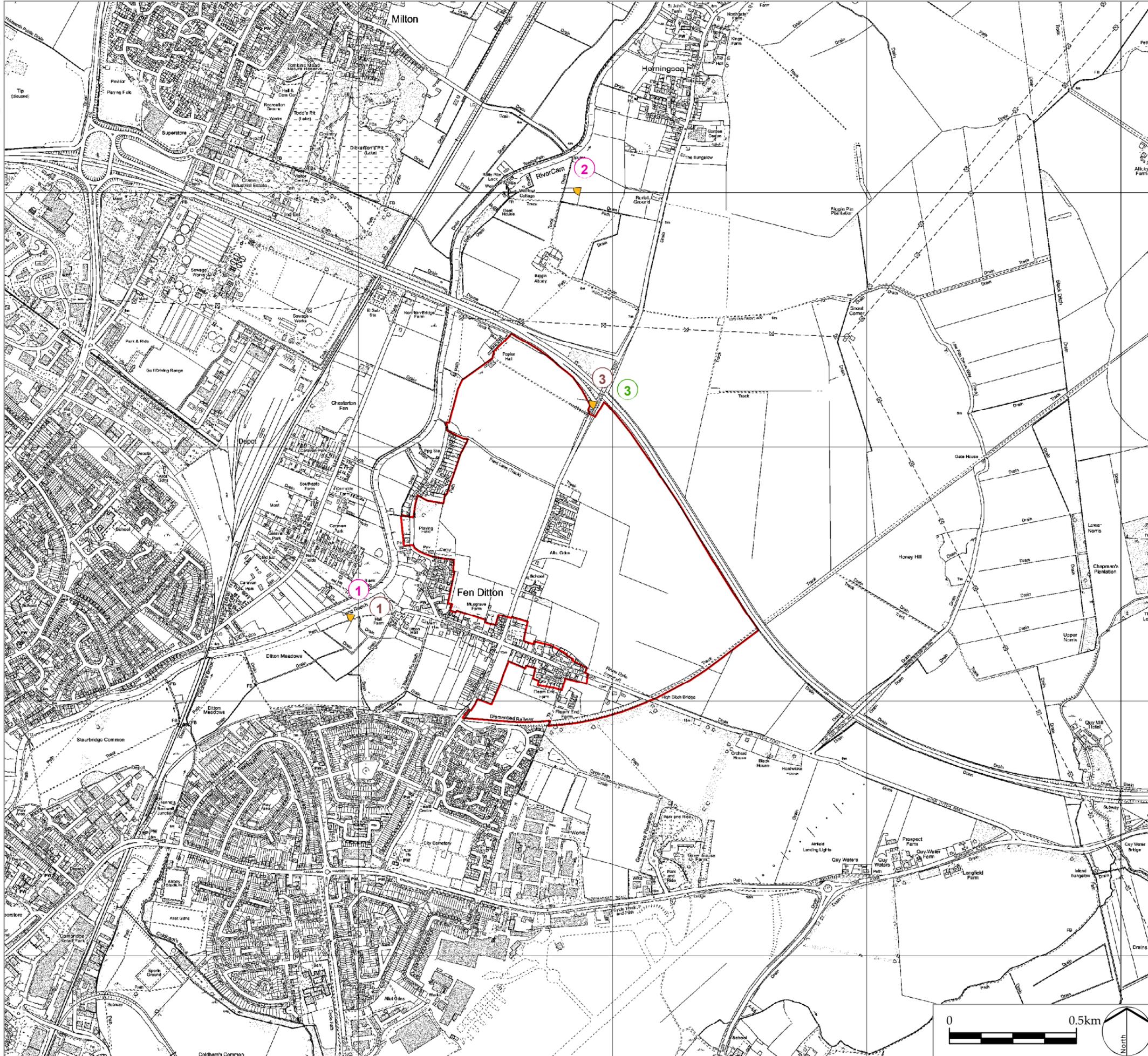
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| ISSUED BY | Peterborough  | T: 01733 310 471 |
| DATE      | November 2015 | DRAWN MPa        |
| SCALE @A3 | 1:20,000      | CHECKED RK       |
| STATUS    | Final         | APPROVED ChC     |

**DWG. NO. 4732\_025**

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 Area measurements for indicative purposes only.

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Sources: Ordnance Survey



LEGEND

-  Study Area
-  Viewpoint

Cambridge Landscape Character Assessment, 2003 -  
Setting and Skyline Views  
N/A

Cambridge Skyline Guidance, 2012 -  
Strategic Viewpoints  
1. Ditton Meadows, south of Fen Ditton  
2. River Cam footpath south of Baits Bite Lock

Cambridge Green Belt Study, 2002 -  
Key Elevated Views and Gateways: First View of Cambridge  
1. Ditton Meadows, south of Fen Ditton - key low-level view  
and first view of Cambridge  
3. A14 junction 34, as it is crossed by the B1047 - key low-level view

2012 Inner Green Belt Boundary Study -  
Significant and Setting Views  
3. A14 junction 34, as it is crossed by the B1047 - setting view

# LDĀDESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
Figure 26:  
North East Cambridge -  
Key Views

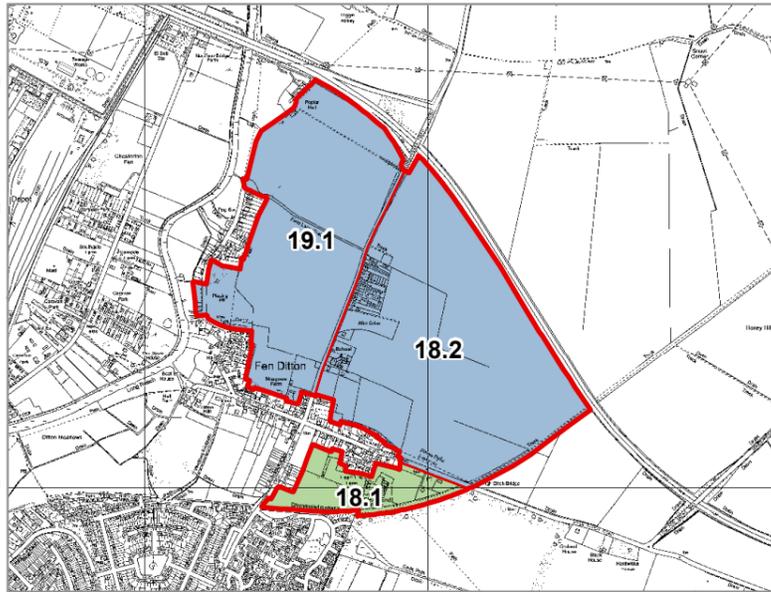
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| STATUS    | Final         | APPROVED ChC     |

**DWG. NO. 4732\_026**

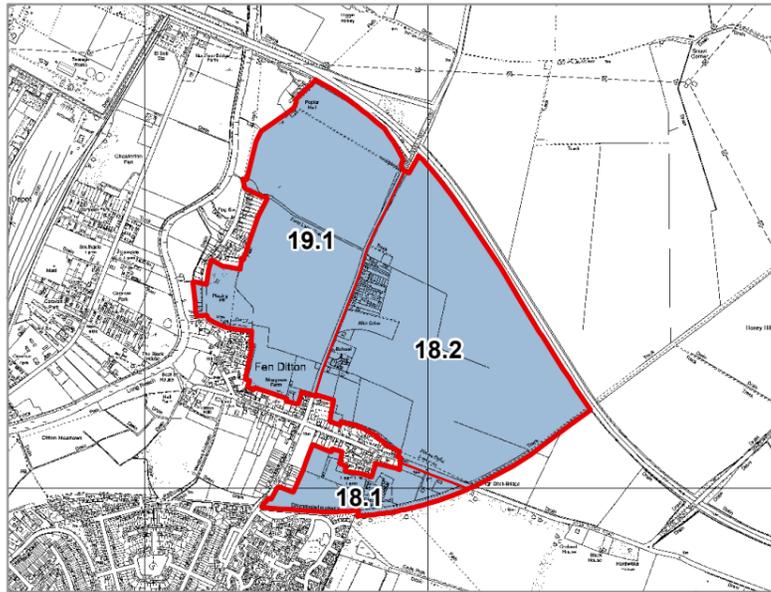
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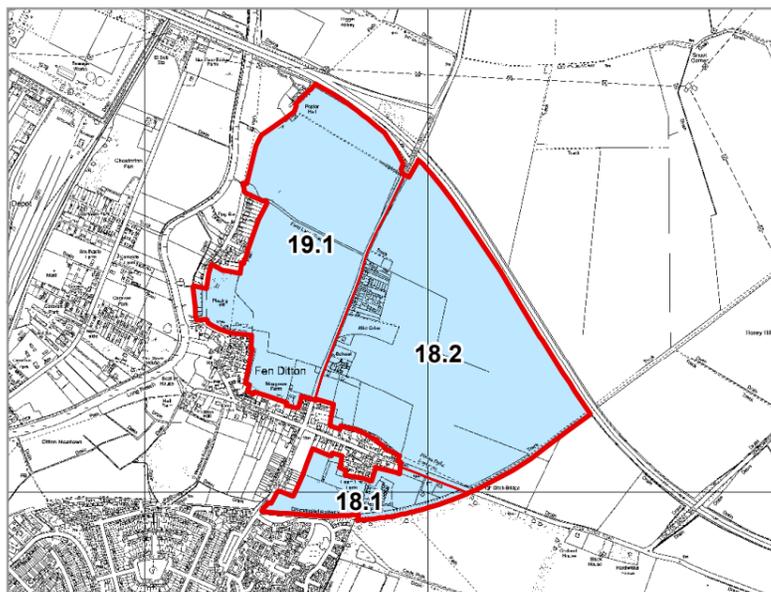
Sources: Ordnance Survey



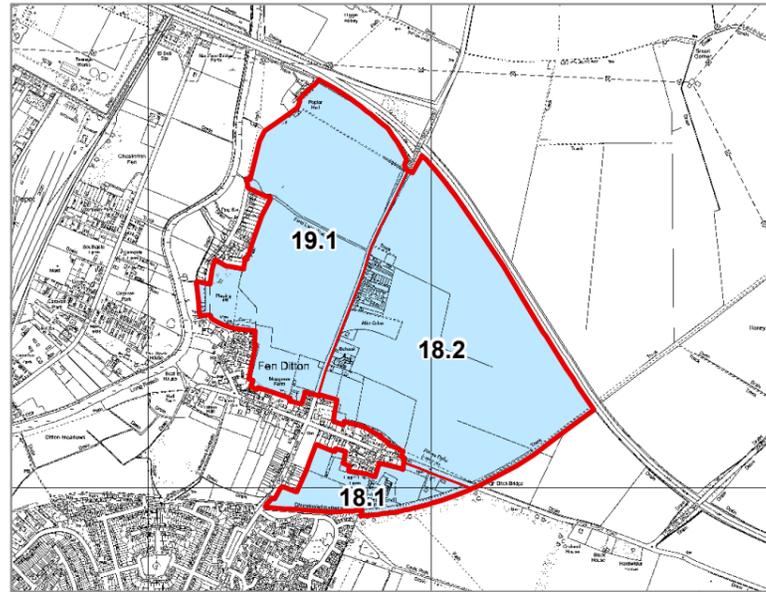
Prevent communities in the environs of Cambridge from merging into one another and with the city



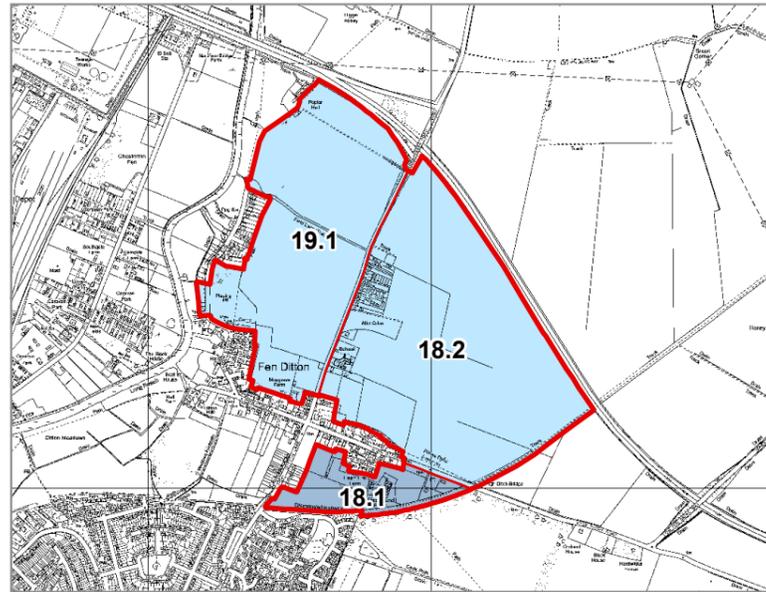
Assist in safeguarding countryside from encroachment



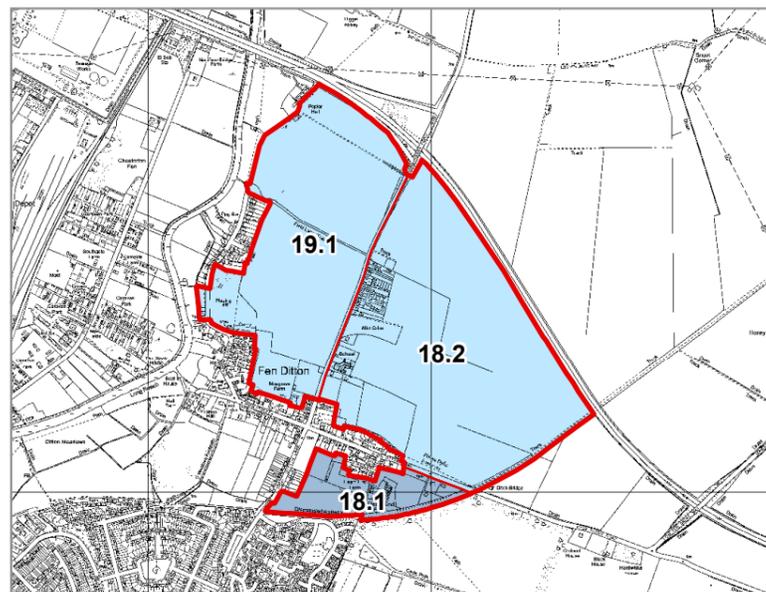
To preserve the setting and special character of Cambridge



Ensure the protection of green corridors running from open countryside into the urban area



To preserve the unique character of Cambridge as a compact, dynamic city with a thriving historic centre

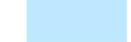


Check unrestricted sprawl of large built up areas

LEGEND

-  Study Area
-  Land Parcel Boundary (with reference number)

Contribution of Land Parcel to Green Belt

-  High
-  Medium
-  Low



# LDĀ DESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
**Figure 27:**  
North East Cambridge - Assessment Summary of Individual Green Belt Purposes

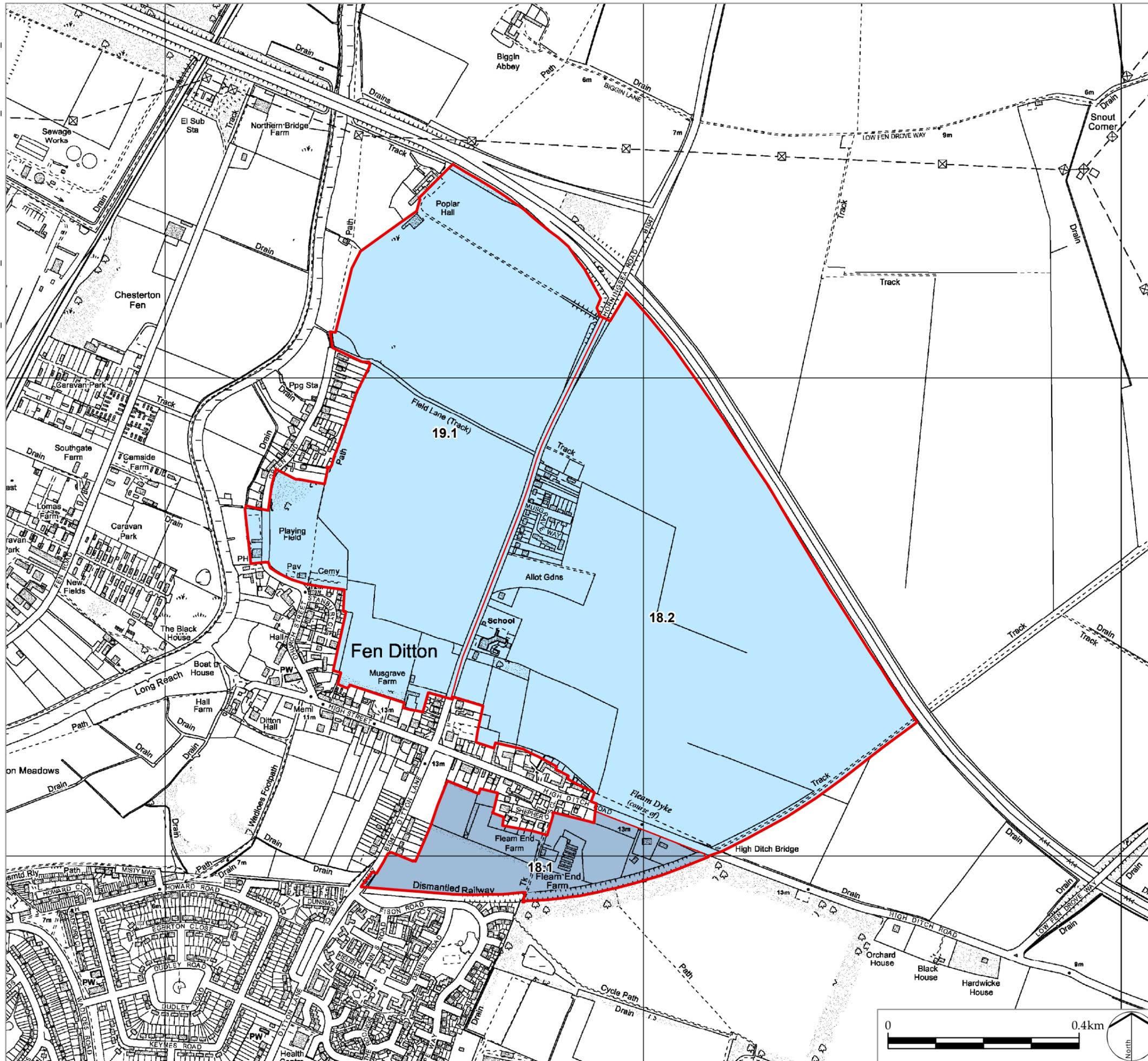
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Sources: Ordnance Survey



LEGEND

- Study Area
- Land Parcel Boundary (with reference number)

Contribution of Land Parcel to Green Belt

- High
- Medium
- Low

# LD A DESIGN

PROJECT TITLE  
CAMBRIDGE GREEN BELT STUDY

DRAWING TITLE  
**Figure 28:  
North East Cambridge - Assessment Summary**

|           |               |                  |
|-----------|---------------|------------------|
| ISSUED BY | Peterborough  | T: 01733 310 471 |
| DATE      | November 2015 | DRAWN MPa        |
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